

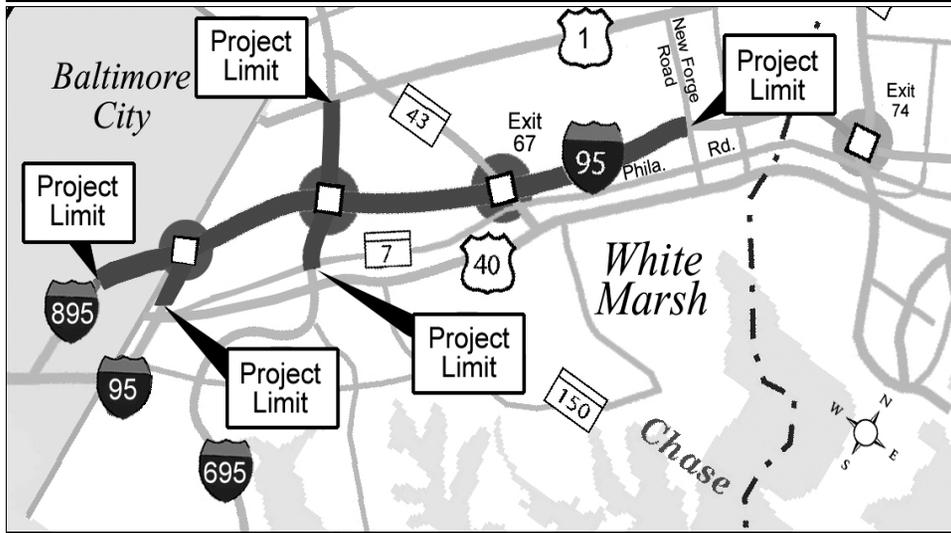
Maryland
Transportation
Authority



MARYLAND TRANSPORTATION AUTHORITY

**MARYLAND TRANSPORTATION AUTHORITY
CAPITAL PROGRAM SUMMARY
(\$ MILLIONS)**

	<u>FY 2011</u>	<u>FY 2012</u>	<u>FY 2013</u>	<u>FY 2014</u>	<u>FY 2015</u>	<u>FY 2016</u>	<u>SIX-YEAR TOTAL</u>
<u>Construction Program</u>							
Major Projects	873.4	502.2	272.8	154.4	30.4	37.5	1,870.8
System Preservation Minor Projects	107.0	98.8	127.9	161.6	135.7	187.5	818.5
<u>Development & Evaluation Program</u>	<u>7.2</u>	<u>0.6</u>	<u>0.6</u>	<u>0.5</u>	<u>-</u>	<u>-</u>	<u>9.0</u>
TOTAL	987.6	601.7	401.3	316.5	166.1	225.0	2,698.3



PROJECT: I-95 John F. Kennedy Memorial Highway - Express Toll Lanes (ETL)

DESCRIPTION: Improve the interchanges with I-895, I-695 and MD 43 and construct two Express Toll Lanes in each direction from I-895 North to north of MD 43 (9.63 miles). (BRAC related.)

PURPOSE & NEED SUMMARY STATEMENT: This segment of I-95 is the most congested section of I-95 in Maryland north of Baltimore City. Currently, I-95, south of MD 43, operates at Level of Service (LOS) F (lowest level of efficiency) during morning and evening rush hours. By 2025, this section is also expected to operate at LOS E (irregular flow with speed variations) and F during weekend peak periods. High congestion levels increase the level of diversion to alternative routes, such as the community-oriented arterials US 1, US 40, and MD 7. If anticipated congestion levels on this segment of I-95 are not addressed, increases in congestion-related accidents would likely occur.

STATE GOALS: Maryland Transportation Plan (MTP) Goals/Selection Criteria:

- Quality of Service
- System Preservation & Performance
- Connectivity for Daily Life
- Safety & Security
- Environmental Stewardship

EXPLANATION: I-95, south of MD 43, experiences congestion during the morning and evening rush hours. By 2025, this section is also expected to experience congestion during weekend peak periods. I-95 is the backbone of the east coast's transportation network and is essential to the movement of people and freight.

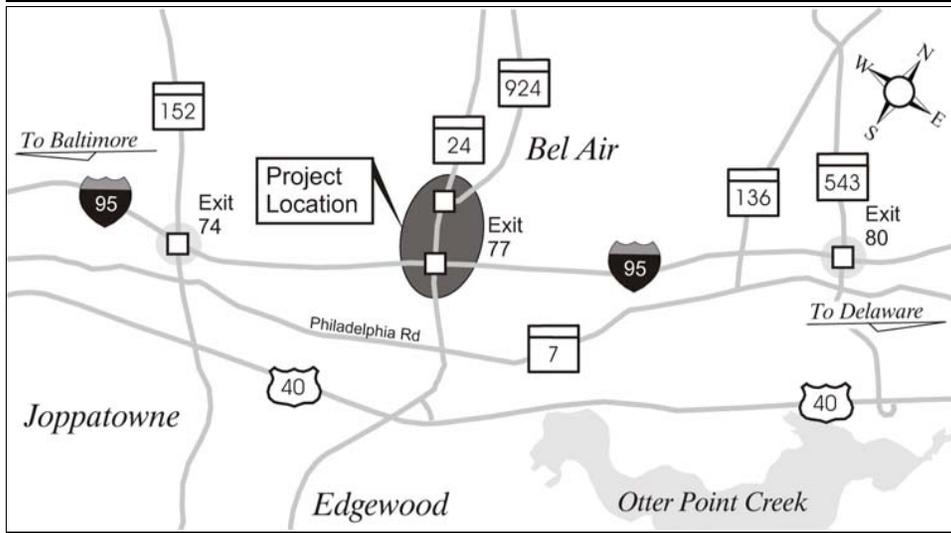
ASSOCIATED IMPROVEMENTS:

- I-95/MD24 Interchange - Construction Program (Line 2)
- I-95 New Maintenance Facility (Line 4)
- I-95 Ft. McHenry Tunnel - Moravia Road to the Tunnel - Construction Program (Line 6)
- I-95 Section 200 - Development and Evaluation Program (Line 33)

STATUS: Engineering, right-of-way acquisition, and construction are underway.

SIGNIFICANT CHANGE FROM FY 2010 - 15 CTP: None.

POTENTIAL FUNDING SOURCE:		<input type="checkbox"/> SPECIAL	<input type="checkbox"/> FEDERAL	<input type="checkbox"/> GENERAL	<input checked="" type="checkbox"/> OTHER					SIX	BALANCE
PHASE	TOTAL ESTIMATED COST (\$000)	EXPEND THRU 2010	CURRENT YEAR 2011	BUDGET YEAR 2012	PROJECTED CASH REQUIREMENTS FOR PLANNING PURPOSES ONLY				YEAR TOTAL	TO COMPLETE	
					.....2013.....	.....2014.....	.....2015.....	.....2016.....		
Planning	6,044	6,044	0	0	0	0	0	0	0	0	
Engineering	135,932	98,667	11,661	10,058	7,773	7,773	0	0	37,265	0	
Right-of-way	52,000	41,986	10,014	0	0	0	0	0	10,014	0	
Construction	800,243	487,421	90,016	57,883	75,788	89,135	0	0	312,822	0	
Total	994,219	634,118	111,691	67,941	83,561	96,908	0	0	360,101	0	
Federal-Aid	0	0	0	0	0	0	0	0	0	0	



PROJECT: I-95 John F. Kennedy Memorial Highway - MD 24 Interchange Improvements (Phase I)

DESCRIPTION: Phase 1, construct improvements to the I-95/MD 24 interchange, including upgrades to MD 24 and the reconstruction of the MD 24/MD 924/Tollgate Road intersection to a grade separated interchange. Phase 2, improvements to the I-95/MD 24 interchange are being developed as part of the I-95 Section 200 project planning study. (BRAC related.)

PURPOSE & NEED SUMMARY STATEMENT: This project will provide improved capacity, operation and safety for the I-95/MD 24 interchange and the MD 24/MD 924/Tollgate Road intersection, which is in close proximity and integral to the I-95/MD 24 interchange operation.

STATE GOALS: Maryland Transportation Plan (MTP) Goals/Selection Criteria:

- Quality of Service
- System Preservation & Performance
- Connectivity for Daily Life
- Safety & Security
- Environmental Stewardship

EXPLANATION: The project will improve capacity and safety for the I-95/MD 24 interchange and the MD 24/MD 924/Tollgate Road intersection. This improvement will address weaving movements on I-95 and MD 24 and improve traffic flow.

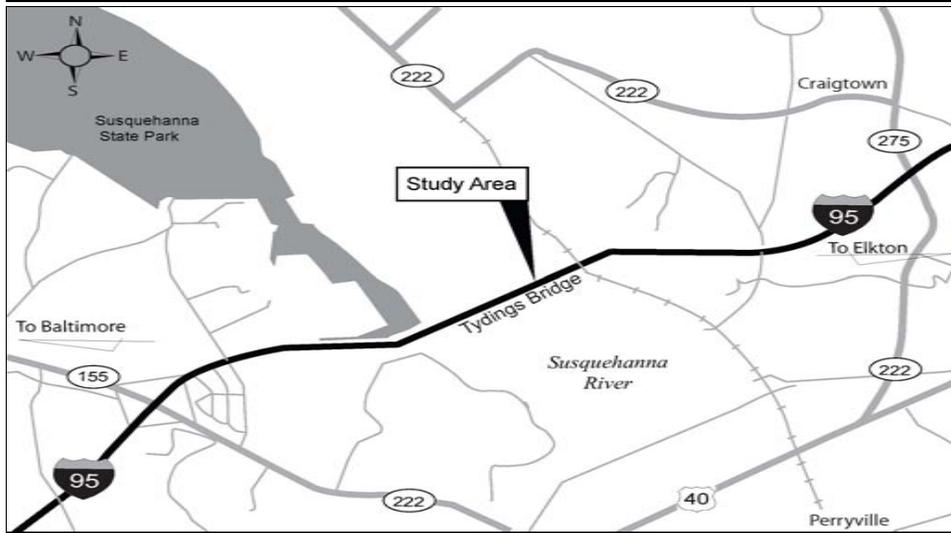
ASSOCIATED IMPROVEMENTS:

- I-95 Express Toll Lanes - Construction Program (Line 1)
- I-95 Section 200 - Development and Evaluation Program (Line 33)

STATUS: Engineering, right-of-way acquisition and construction are underway.

SIGNIFICANT CHANGE FROM FY 2010 - 15 CTP: None.

POTENTIAL FUNDING SOURCE:		<input checked="" type="checkbox"/> SPECIAL		<input type="checkbox"/> FEDERAL		<input type="checkbox"/> GENERAL		<input checked="" type="checkbox"/> OTHER			
PHASE	TOTAL		CURRENT YEAR	BUDGET YEAR	PROJECTED CASH REQUIREMENTS FOR PLANNING PURPOSES ONLY				SIX YEAR TOTAL	BALANCE TO COMPLETE	
	ESTIMATED COST (\$000)	EXPEND THRU 2010			2011	20122013.....2014.....		2015.....	.....2016.....
Planning	0	0	0	0	0	0	0	0	0	0	0
Engineering	9,951	8,286	1,665	0	0	0	0	0	1,665	0	0
Right-of-way	4,000	2,076	1,924	0	0	0	0	0	1,924	0	0
Construction	46,612	20,867	23,745	2,000	0	0	0	0	25,745	0	0
Total	60,563	31,229	27,334	2,000	0	0	0	0	29,334	0	0
Federal-Aid	0	0	0	0	0	0	0	0	0	0	0



PROJECT: I-95 John F. Kennedy Memorial Highway - Underwater Repairs at Tydings Bridge

DESCRIPTION: Rehabilitate pier foundations and provide pier scour protection.

PURPOSE & NEED SUMMARY STATEMENT: Pier foundations in the river exhibit advanced wear and analysis shows scour protection is needed.

STATE GOALS: Maryland Transportation Plan (MTP) Goals/Selection Criteria:

- Quality of Service
- System Preservation & Performance
- Connectivity for Daily Life
- Safety & Security
- Environmental Stewardship

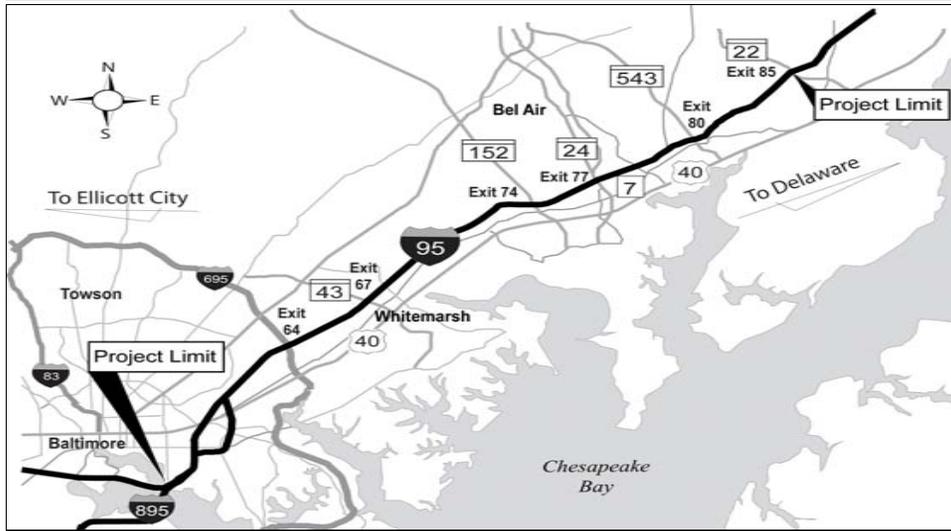
ASSOCIATED IMPROVEMENTS: None.

EXPLANATION: Pier foundations were found to have advanced wear and require scour protection.

STATUS: Engineering is underway. Construction to begin in FY 2012.

POTENTIAL FUNDING SOURCE:		<input type="checkbox"/> SPECIAL <input type="checkbox"/> FEDERAL <input type="checkbox"/> GENERAL <input checked="" type="checkbox"/> OTHER								
PHASE	TOTAL				PROJECTED CASH REQUIREMENTS				SIX YEAR TOTAL	BALANCE TO COMPLETE
	ESTIMATED COST (\$000)	EXPEND THRU 2010	CURRENT YEAR 2011	BUDGET YEAR 2012	FOR PLANNING PURPOSES ONLY	FOR PLANNING PURPOSES ONLY	FOR PLANNING PURPOSES ONLY	FOR PLANNING PURPOSES ONLY		
Planning	0	0	0	0	0	0	0	0	0	0
Engineering	1,850	255	542	628	425	0	0	0	1,595	0
Right-of-way	0	0	0	0	0	0	0	0	0	0
Construction	52,910	0	0	22,788	30,122	0	0	0	52,910	0
Total	54,760	255	542	23,416	30,547	0	0	0	54,505	0
Federal-Aid	0	0	0	0	0	0	0	0	0	0

SIGNIFICANT CHANGE FROM FY 2010 - 15 CTP: Cost increased by \$13 million due to increased scope including enhanced rehabilitation and protection through thicker encasements and increased environmental mitigation work.



PROJECT: I-95 John F. Kennedy Memorial Highway - New Maintenance Facility

DESCRIPTION: The project includes the planning and right of way acquisition for a new maintenance facility along the I-95 John F. Kennedy Memorial Highway. Construction funding is included to purchase additional equipment for maintenance and operations of the Express Toll Lanes (ETL) and associated interchange improvements only.

PURPOSE & NEED SUMMARY STATEMENT: A new maintenance facility will be required to replace the existing JFK Maintenance 1 because Section 200 Project will displace this facility. The new maintenance Facility will cover the current service area for JFK Maintenance 1 as well as I-95 Master Plan Improvements, including the ETL's and associated interchange improvements.

STATE GOALS : Maryland Transportation Plan (MTP) Goals/Selection Criteria:

- Quality of Service
- System Preservation & Performance
- Connectivity for Daily Life
- Safety & Security
- Environmental Stewardship

EXPLANATION: A new maintenance facility will be needed when the I-95 Section 200 improvements are implemented. This project will acquire a site for a new maintenance facility.

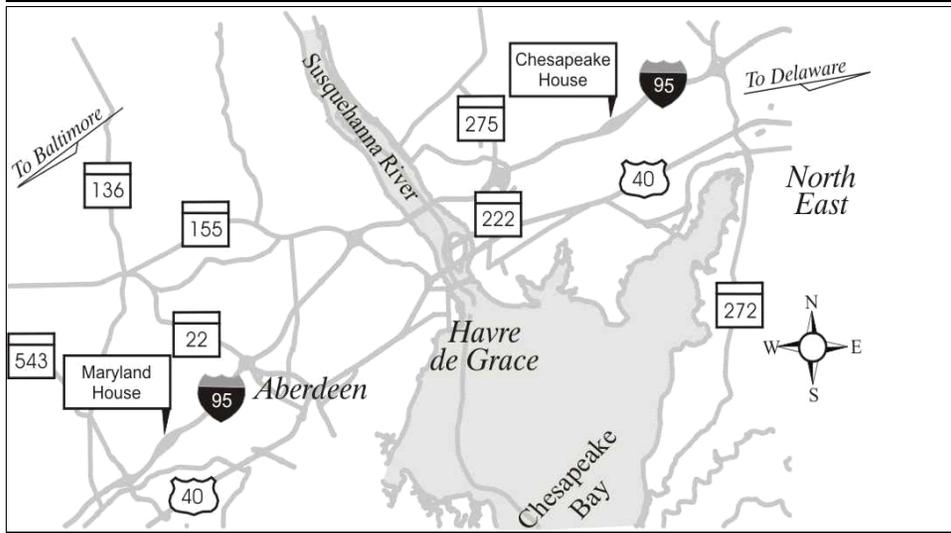
ASSOCIATED IMPROVEMENTS:

- I-95 Express Toll Lanes - Construction Program (Line 1)
- I-95 Section 200 - Development & Evaluation Program (Line 33)

STATUS: Planning and right of way acquisition are underway. Equipment and vehicle acquisition (Construction phase) is underway.

SIGNIFICANT CHANGE FROM FY 2010 - 15 CTP: Cost reduced by \$6.5 million due to elimination of engineering funding and reduced land costs.

POTENTIAL FUNDING SOURCE:											
<input type="checkbox"/> SPECIAL <input type="checkbox"/> FEDERAL <input type="checkbox"/> GENERAL <input checked="" type="checkbox"/> OTHER											
PHASE	TOTAL				PROJECTED CASH REQUIREMENTS				SIX YEAR TOTAL	BALANCE TO COMPLETE	
	ESTIMATED COST (\$000)	EXPEND THRU 2010	CURRENT YEAR 2011	BUDGET YEAR 2012	FOR PLANNING PURPOSES ONLY	FOR PLANNING PURPOSES ONLY	FOR PLANNING PURPOSES ONLY	FOR PLANNING PURPOSES ONLY			
					.....2013.....	.....2014.....	.....2015.....	.....2016.....		
Planning	451	4	224	223	0	0	0	0	447	0	
Engineering	4	4	0	0	0	0	0	0	0	0	
Right-of-way	2,090	1,137	953	0	0	0	0	0	953	0	
Construction	8,400	771	744	1,622	2,561	2,702	0	0	7,629	0	
Total	10,945	1,916	1,921	1,845	2,561	2,702	0	0	9,029	0	
Federal-Aid	0	0	0	0	0	0	0	0	0	0	



PROJECT: I-95 John F. Kennedy Memorial Highway - Travel Plazas Redevelopment

DESCRIPTION: Replace and/or rehabilitate the structures and the operations of the concessions. An RFP is being re-developed for a design, build, operate, maintain and finance contract for the two I-95 travel plazas.

PURPOSE & NEED SUMMARY STATEMENT: The Maryland House and Chesapeake House Travel Plazas have aged to the point in which redesign, and/or reconstruction is necessary to adequately meet public demand over the next 30 years.

STATE GOALS : Maryland Transportation Plan (MTP) Goals/Selection Criteria:

- Quality of Service
- System Preservation & Performance
- Connectivity for Daily Life
- Safety & Security
- Environmental Stewardship

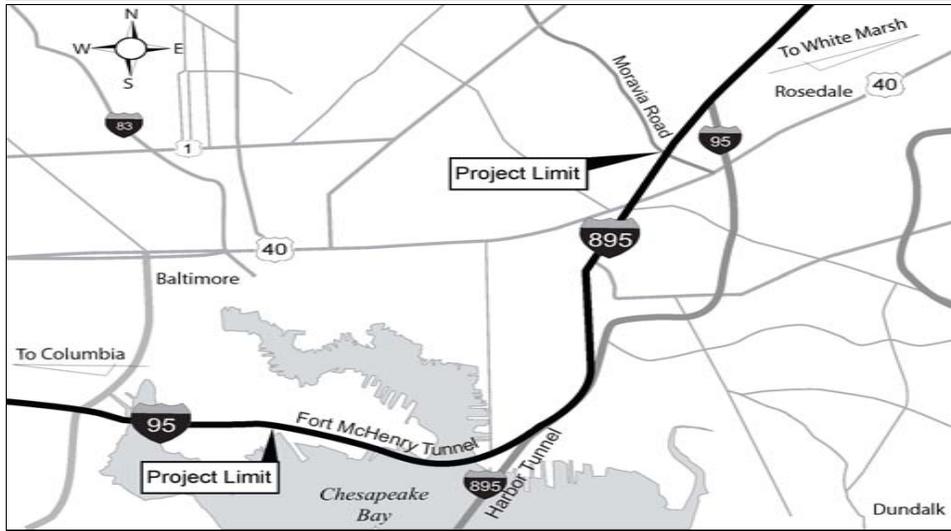
ASSOCIATED IMPROVEMENTS:
None.

EXPLANATION: The structures were built in the 1960's and 70's and are nearing the end of their design life. Age and condition require major rehab which could be as costly as reconstruction efforts.

STATUS: Moved from the D&E Program to the Construction Program. The RFP has been cancelled and will be re-issued at a later date. Construction funding is to be financed by the winning bidder.

POTENTIAL FUNDING SOURCE:		<input type="checkbox"/> SPECIAL		<input type="checkbox"/> FEDERAL		<input type="checkbox"/> GENERAL		<input checked="" type="checkbox"/> OTHER			
PHASE	TOTAL ESTIMATED COST (\$000)	EXPEND THRU 2010	CURRENT YEAR 2011	BUDGET YEAR 2012	PROJECTED CASH REQUIREMENTS FOR PLANNING PURPOSES ONLY				SIX YEAR TOTAL	BALANCE TO COMPLETE	
					....2013....	....2014....	....2015....	....2016....		
Planning	4,154	1,438	1,000	600	600	516	0	0	2,716	0	
Engineering	1,870	1,870	0	0	0	0	0	0	0	0	
Right-of-way	250	0	250	0	0	0	0	0	250	0	
Construction	0	0	0	0	0	0	0	0	0	0	
Total	6,274	3,308	1,250	600	600	516	0	0	2,966	0	
Federal-Aid	0	0	0	0	0	0	0	0	0	0	

SIGNIFICANT CHANGE FROM FY 2010 - 15 CTP: Added right-of-way funding to purchase easement for water and sewer services. The schedule will be delayed to allow for a re-issuance of the RFP.



PROJECT: I-95 Fort McHenry Tunnel - Moravia Road to the Tunnel Modifications

DESCRIPTION: The project limits extend from the southern end of the I-95, ETL Project (Moravia Road) to the Fort McHenry Tunnel Toll Plaza. Phase 1 modifications from Moravia Road to Eastern Avenue include grinding and overlaying to provide four continuous through lanes (lane continuity); Phase 2 from Eastern Avenue to Fort McHenry Tunnel includes re-striping to provide four continuous through lanes (lane continuity).

PURPOSE & NEED SUMMARY STATEMENT: The existing typical section of I-95 from Moravia Road to the FMT is as follows: three lanes per direction from I-895 to Moravia Road; four lanes per direction from Moravia Road to O'Donnell Street; three lanes per direction from O'Donnell Street to Boston Street; and four lanes per direction from Boston Street to the Fort McHenry Tunnel. This project will provide southbound lane continuity from the southern limits of the ETL's to the tunnel.

STATE GOALS : Maryland Transportation Plan (MTP) Goals/Selection Criteria:

- Quality of Service
- System Preservation & Performance
- Connectivity for Daily Life
- Safety & Security
- Environmental Stewardship

EXPLANATION: This project will provide southbound lane continuity from the southern limits of the ETL's to the Fort McHenry Tunnel. This will improve traffic flow on this segment of I-95.

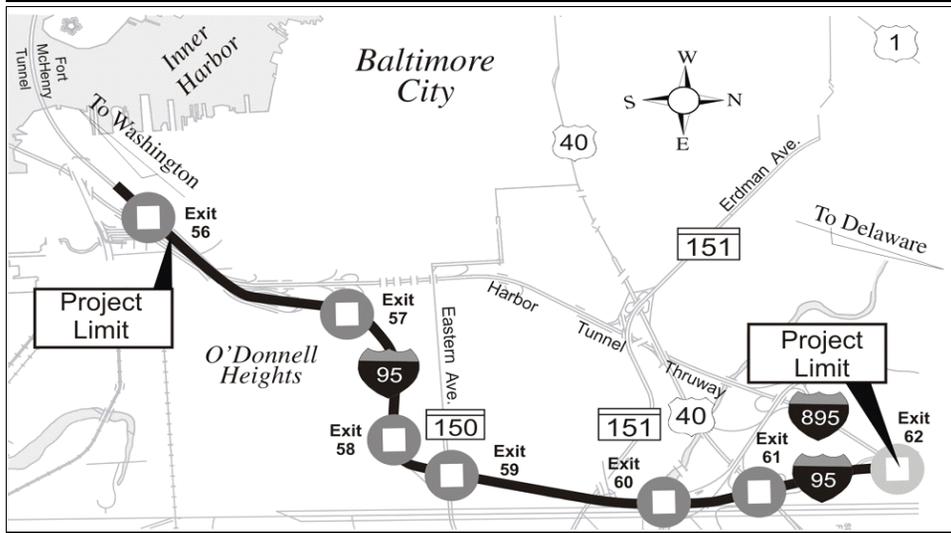
ASSOCIATED IMPROVEMENTS:

- I-95 Express Toll Lanes - Construction Program (Line 1)
- I-95 Ft. McHenry Tunnel - Rehabilitation North of Tunnel - Construction Program (Line 7)
- I-95 Ft. McHenry Tunnel - Rehab. from Joh Ave. to Washington Blvd. - Construction Program (Line 8)
- I-95 Ft. McHenry Tunnel - Higher Speed Toll Plaza Modifications - Construction Program (Line 9)

STATUS: Planning is complete. Engineering is underway. Construction for Phase I southbound began in August 2010.

POTENTIAL FUNDING SOURCE:		<input type="checkbox"/> SPECIAL <input type="checkbox"/> FEDERAL <input type="checkbox"/> GENERAL <input checked="" type="checkbox"/> OTHER									
PHASE	TOTAL				PROJECTED CASH REQUIREMENTS				SIX YEAR TOTAL	BALANCE TO COMPLETE	
	ESTIMATED COST (\$000)	EXPEND THRU 2010	CURRENT YEAR 2011	BUDGET YEAR 2012	FOR PLANNING PURPOSES ONLY						
					.....2013.....	.....2014.....	.....2015.....	.....2016.....		
Planning	555	555	0	0	0	0	0	0	0	0	
Engineering	1,494	1,087	407	0	0	0	0	0	407	0	
Right-of-way	0	0	0	0	0	0	0	0	0	0	
Construction	11,309	0	3,508	7,801	0	0	0	0	11,309	0	
Total	13,358	1,642	3,915	7,801	0	0	0	0	11,716	0	
Federal-Aid	0	0	0	0	0	0	0	0	0	0	

SIGNIFICANT CHANGE FROM FY 2010 - 15 CTP: Construction start delayed to FY 2011 due to additional environmental coordination. Reduced cost by \$2.2 million based on actual bid prices.



PROJECT: I-95 Fort McHenry Tunnel - Bridge, Roadway and Signage Rehabilitation North of Tunnel

DESCRIPTION: Rehabilitate I-95 north of the Tunnel to I-895. Includes resurfacing of 34 bridge decks and related structural repairs; resurfacing and safety improvements of roadways; replacing and upgrading of existing signing; and inspecting and repairing of high mast light poles and sign structures.

PURPOSE & NEED SUMMARY STATEMENT: Bridge decks and roadways have not been resurfaced since their opening in 1976 and they exhibit various degrees of wear. Signs and other safety features need to be brought up to the latest standards.

STATE GOALS: Maryland Transportation Plan (MTP) Goals/Selection Criteria:

- Quality of Service
- System Preservation & Performance
- Connectivity for Daily Life
- Safety & Security
- Environmental Stewardship

EXPLANATION: Bridge decks and roadways exhibit various degrees of wear. Signs and other safety features need to be brought up to the latest standards.

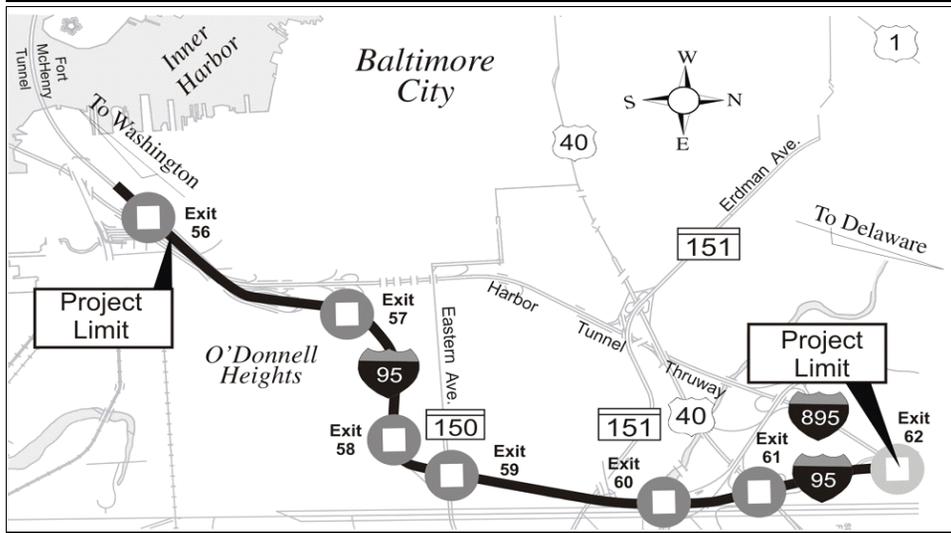
ASSOCIATED IMPROVEMENTS:

- I-95 Ft. McHenry Tunnel - Moravia Road to Tunnel Modifications - Construction Program (Line 6)
- I-95 Ft. McHenry Tunnel - Rehab. from Joh Ave. to Washington Blvd. - Construction Program (Line 8)
- I-95 Ft. McHenry Tunnel - Higher Speed Toll Plaza Modifications - Construction Program (Line 9)
- I-95 Ft. McHenry Tunnel - Rehabilitate Concrete Tunnel Deck - Construction Program (Line 10)

STATUS: Bridge and roadway rehabilitation are complete. Signing and lighting work is underway.

PHASE	TOTAL				PROJECTED CASH REQUIREMENTS FOR PLANNING PURPOSES ONLY				SIX YEAR TOTAL	BALANCE TO COMPLETE
	ESTIMATED COST (\$000)	EXPEND THRU 2010	CURRENT YEAR 2011	BUDGET YEAR 2012	.....2013.....	.....2014.....	.....2015.....	.....2016.....		
Planning	0	0	0	0	0	0	0	0	0	0
Engineering	6,483	6,203	130	150	0	0	0	0	280	0
Right-of-way	0	0	0	0	0	0	0	0	0	0
Construction	33,470	24,820	3,767	4,883	0	0	0	0	8,650	0
Total	39,953	31,023	3,897	5,033	0	0	0	0	8,930	0
Federal-Aid	0	0	0	0	0	0	0	0	0	0

SIGNIFICANT CHANGE FROM FY 2010 - 15 CTP: Construction is underway. Cost reduced by \$6.8 million based on actual bid prices.



PROJECT: I-95 Fort McHenry Tunnel - Bridge, Roadway and Signage Rehabilitation from Joh Avenue to Washington Boulevard

DESCRIPTION: Rehabilitation of I-95 South of the Fort McHenry Tunnel. Scope of work includes overlaying the roadway section, excluding bridge decks, between Joh Avenue and Washington Boulevard and replacement and upgrades of existing signing; miscellaneous safety improvements; and inspection and repair of high mast light poles and sign structures.

PURPOSE & NEED SUMMARY STATEMENT: Roadway has not been resurfaced since its opening in 1976 and it exhibits various degrees of wear. Signing, lighting and other safety features need to be brought up to the latest standards.

STATE GOALS: Maryland Transportation Plan (MTP) Goals/Selection Criteria:

- Quality of Service
- System Preservation & Performance
- Connectivity for Daily Life
- Safety & Security
- Environmental Stewardship

EXPLANATION: Roadway exhibits various degrees of wear. Signing, lighting and other safety features need to be brought up to the latest standards.

ASSOCIATED IMPROVEMENTS:

- I-95 Ft. McHenry Tunnel - Moravia Road to Tunnel Modifications - Construction Program (Line 6)
- I-95 Ft. McHenry Tunnel - Rehabilitation North of the Tunnel - Construction Program (Line 7)
- I-95 Ft. McHenry Tunnel - Higher Speed Toll Plaza Modifications - Construction Program (Line 9)
- I-95 Ft. McHenry Tunnel - Rehabilitate Concrete Tunnel Deck - Construction Program (Line 10)

STATUS: Construction is underway.

PHASE	TOTAL				PROJECTED CASH REQUIREMENTS FOR PLANNING PURPOSES ONLY				SIX YEAR TOTAL	BALANCE TO COMPLETE
	ESTIMATED COST (\$000)	EXPEND THRU 2010	CURRENT YEAR 2011	BUDGET YEAR 2012	.....2013.....	.....2014.....	.....2015.....	.....2016.....		
Planning	0	0	0	0	0	0	0	0	0	0
Engineering	4,177	1,635	1,714	828	0	0	0	0	2,542	0
Right-of-way	0	0	0	0	0	0	0	0	0	0
Construction	33,976	17,746	7,176	9,054	0	0	0	0	16,230	0
Total	38,153	19,381	8,890	9,882	0	0	0	0	18,772	0
Federal-Aid	0	0	0	0	0	0	0	0	0	0

SIGNIFICANT CHANGE FROM FY 2010 - 15 CTP: None.



PROJECT: I-95 Fort McHenry Tunnel - Higher Speed Toll Plaza Modifications

DESCRIPTION: Installed higher speed toll lanes (northbound/southbound) at Fort McHenry Tunnel toll plaza. This improvement enables E-Z Pass customers to pass through the toll plazas at 30 mph (similar to FSK Bridge toll plaza).

PURPOSE & NEED SUMMARY STATEMENT: This project has improved traffic operations and capacity at the toll plaza.

STATE GOALS: Maryland Transportation Plan (MTP) Goals/Selection Criteria:

- Quality of Service
- System Preservation & Performance
- Connectivity for Daily Life
- Safety & Security
- Environmental Stewardship

EXPLANATION: This project has improved traffic flow at the toll plaza.

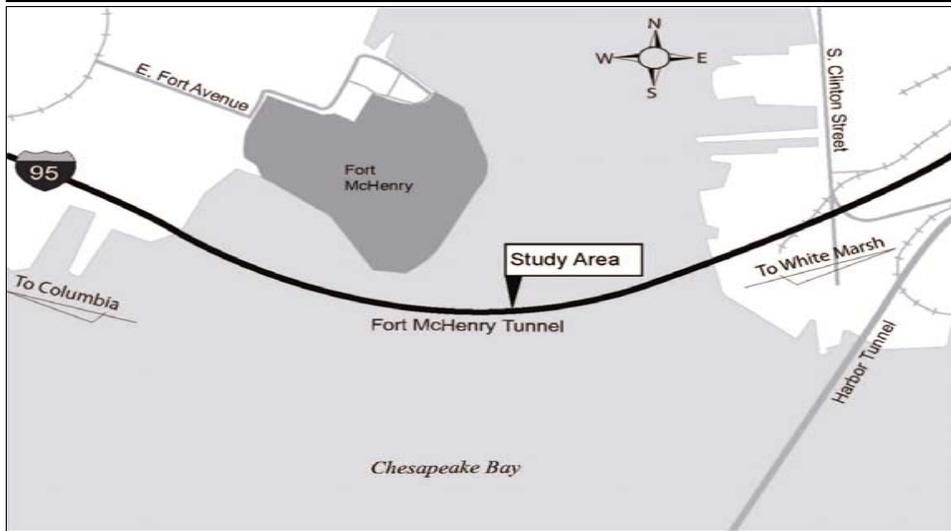
ASSOCIATED IMPROVEMENTS:

- I-95 Ft. McHenry Tunnel - Moravia Road to Tunnel Modifications - Construction Program (Line 6)
- I-95 Ft. McHenry Tunnel - Rehabilitation North of Tunnel - Construction Program (Line 7)
- I-95 Ft. McHenry Tunnel - Rehab. from Joh Ave. to Washington Blvd. - Construction Program (Line 8)
- I-95 Ft. McHenry Tunnel - Rehabilitate Concrete Tunnel Deck - Construction Program (Line 10)

STATUS: Construction is complete.

PHASE	POTENTIAL FUNDING SOURCE:									
	<input type="checkbox"/> SPECIAL <input type="checkbox"/> FEDERAL <input type="checkbox"/> GENERAL <input checked="" type="checkbox"/> OTHER									
	TOTAL ESTIMATED COST (\$000)	EXPEND THRU 2010	CURRENT YEAR 2011	BUDGET YEAR 2012	PROJECTED CASH REQUIREMENTS FOR PLANNING PURPOSES ONLY				SIX YEAR TOTAL	BALANCE TO COMPLETE
				.....2013.....	.....2014.....	.....2015.....	.....2016.....			
Planning	0	0	0	0	0	0	0	0	0	0
Engineering	2,029	2,029	0	0	0	0	0	0	0	0
Right-of-way	0	0	0	0	0	0	0	0	0	0
Construction	13,605	13,583	22	0	0	0	0	0	22	0
Total	15,634	15,612	22	0	0	0	0	0	22	0
Federal-Aid	0	0	0	0	0	0	0	0	0	0

SIGNIFICANT CHANGE FROM FY 2010 - 15 CTP: Cost increased by \$0.9 million due to increased engineering and inspection costs.



PROJECT: I-95 Fort McHenry Tunnel - Rehabilitate Concrete Tunnel Deck

DESCRIPTION: Rehabilitate the tunnel deck for all four bores.

PURPOSE & NEED SUMMARY STATEMENT: The bottom of the tunnel deck is showing signs of wear throughout the length of the tunnel. This improvement will significantly increase the useful life of the tunnel deck.

STATE GOALS: Maryland Transportation Plan (MTP) Goals/Selection Criteria:

- Quality of Service
- System Preservation & Performance
- Connectivity for Daily Life
- Safety & Security
- Environmental Stewardship

EXPLANATION: The tunnel deck shows signs of wear. This improvement will extend the useful life of the tunnel deck.

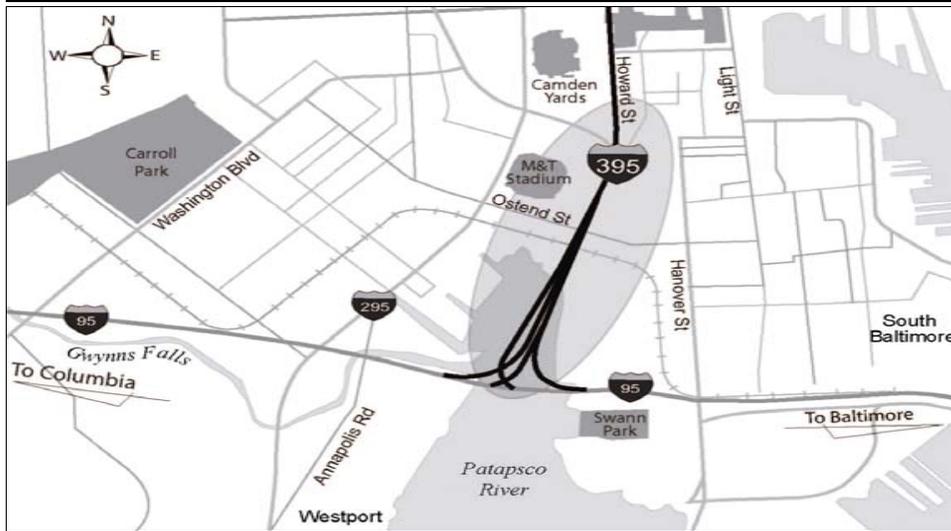
ASSOCIATED IMPROVEMENTS:

- I-95 Ft. McHenry Tunnel - Moravia Road to Tunnel Modifications - Construction Program (Line 6)
- I-95 Ft. McHenry Tunnel - Rehabilitation North of Tunnel - Construction Program (Line 7)
- I-95 Ft. McHenry Tunnel - Rehab. from Joh Ave. to Washington Blvd. - Construction Program (Line 8)
- I-95 Ft. McHenry Tunnel - Higher Speed Toll Plaza Modifications - Construction Program (Line 9)

STATUS: Engineering is underway. Construction to begin in FY 2011.

SIGNIFICANT CHANGE FROM FY 2010 - 15 CTP: None.

POTENTIAL FUNDING SOURCE:										
<input type="checkbox"/> SPECIAL <input type="checkbox"/> FEDERAL <input type="checkbox"/> GENERAL <input checked="" type="checkbox"/> OTHER										
PHASE	TOTAL		CURRENT YEAR	BUDGET YEAR	PROJECTED CASH REQUIREMENTS FOR PLANNING PURPOSES ONLY				SIX YEAR TOTAL	BALANCE TO COMPLETE
	ESTIMATED COST (\$000)	EXPEND THRU 2010			2011	20122013....2014....			....2015....	....2016....
Planning	0	0	0	0	0	0	0	0	0	0
Engineering	919	477	442	0	0	0	0	0	442	0
Right-of-way	0	0	0	0	0	0	0	0	0	0
Construction	21,414	1	1,262	15,886	4,265	0	0	0	21,413	0
Total	22,333	478	1,704	15,886	4,265	0	0	0	21,855	0
Federal-Aid	0	0	0	0	0	0	0	0	0	0



PROJECT: I-95 Fort McHenry Tunnel - I-395 Repair and Spot Paint Beam Ends, Deck Seal and Joint Repair

DESCRIPTION: Repair and paint beam ends, seal concrete deck, and repair joints.

PURPOSE & NEED SUMMARY STATEMENT: Beam ends, bridge deck and roadway joints exhibit various degrees of wear. This improvement will correct current wear and prevent further wear of the concrete deck and steel beams.

STATE GOALS : Maryland Transportation Plan (MTP) Goals/Selection Criteria:

- Quality of Service
- System Preservation & Performance
- Connectivity for Daily Life
- Safety & Security
- Environmental Stewardship

EXPLANATION: Repairs are needed to correct current wear and to prevent further wear of the concrete deck and steel beams.

ASSOCIATED IMPROVEMENTS:

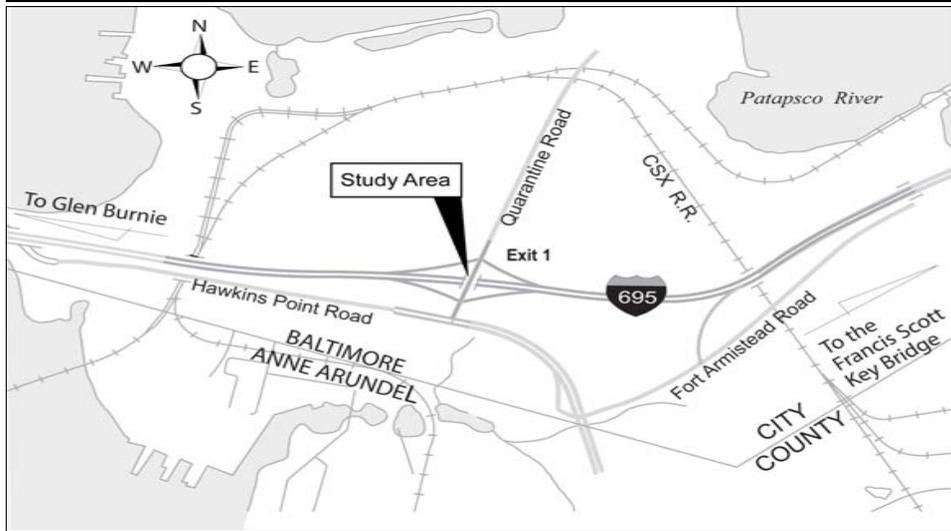
- I-95 Ft. McHenry Tunnel - Moravia Road to Tunnel Modifications - Construction Program (Line 6)
- I-95 Ft. McHenry Tunnel - Rehabilitation North of Tunnel - Construction Program (Line 7)
- I-95 Ft. McHenry Tunnel - Rehab. from Joh Ave. to Washington Blvd. - Construction Program (Line 8)
- I-95 Ft. McHenry Tunnel - Higher Speed Toll Plaza Modifications - Construction Program (Line 9)

STATUS: Engineering is underway. Construction to begin in FY 2012.

POTENTIAL FUNDING SOURCE:		<input type="checkbox"/> SPECIAL <input type="checkbox"/> FEDERAL <input type="checkbox"/> GENERAL <input checked="" type="checkbox"/> OTHER								
PHASE	TOTAL				PROJECTED CASH REQUIREMENTS				SIX YEAR TOTAL	BALANCE TO COMPLETE
	ESTIMATED COST (\$000)	EXPEND THRU 2010	CURRENT YEAR 2011	BUDGET YEAR 2012	FOR PLANNING PURPOSES ONLY					
					.....2013.....	.....2014.....	.....2015.....	.....2016.....		
Planning	0	0	0	0	0	0	0	0	0	0
Engineering	2,062	41	281	617	647	317	159	0	2,021	0
Right-of-way	0	0	0	0	0	0	0	0	0	0
Construction	17,029	0	0	779	3,020	4,998	5,500	2,732	17,029	0
Total	19,091	41	281	1,396	3,667	5,315	5,659	2,732	19,050	0
Federal-Aid	0	0	0	0	0	0	0	0	0	0

SIGNIFICANT CHANGE FROM FY 2010 - 15 CTP:

Construction start delayed to FY 2012 due to sequencing of contracts.



PROJECT: MD 695 Francis Scott Key Bridge - Interchange Improvements at Quarantine Road

DESCRIPTION: Improvements to Quarantine Road interchange will include widening of the ramps and upgraded traffic signals; permanent closure of the ramp from Quarantine Road to the Outer Loop and redirection of traffic to the Fort Armistead ramp; widening of the approach roadways and restriping the Quarantine Road bridge to four lanes; a new storm water management facility to control storm water pollution; new roadway signing, lighting, pavement markings, landscaping and reforestation.

PURPOSE & NEED SUMMARY STATEMENT: This project will improve safety by addressing constraints at the existing MD 695 - Quarantine Road interchange that result in ramp queues that extend onto MD 695.

STATE GOALS: Maryland Transportation Plan (MTP) Goals/Selection Criteria:

- Quality of Service
- System Preservation & Performance
- Connectivity for Daily Life
- Safety & Security
- Environmental Stewardship

EXPLANATION: This project will improve safety by addressing constraints at the existing MD 695 - Quarantine Road interchange. This improvement will eliminate existing queues, which extend onto mainline MD 695.

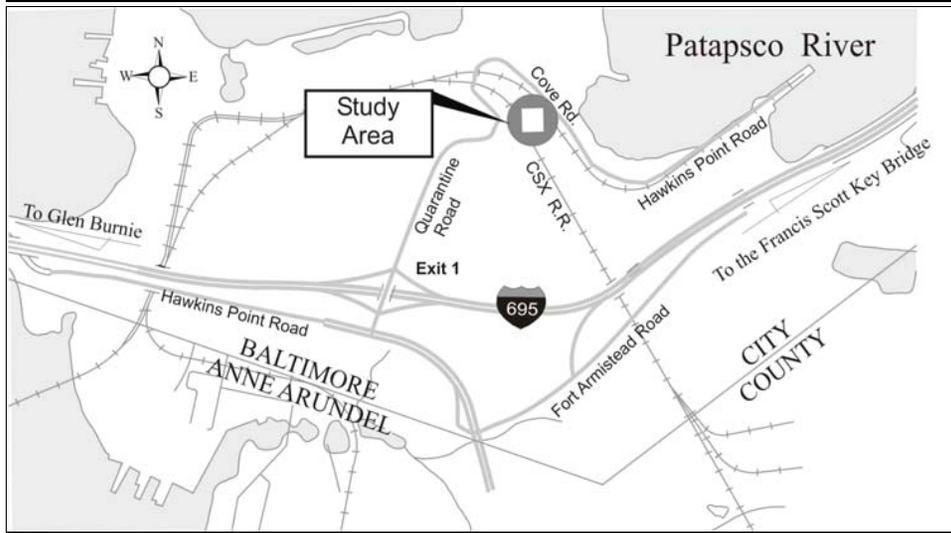
ASSOCIATED IMPROVEMENTS:

- MD 695 Francis Scott Key Bridge - Replace Curtis Creek Grid Deck - Construction Program (Line 14)
- MD 695 Francis Scott Key Bridge - Repair Approach Spans and Paint Fascia Beams - Construction Program (Line 15)

STATUS: Engineering and right-of-way acquisition are complete. Construction is underway.

POTENTIAL FUNDING SOURCE:		<input type="checkbox"/> SPECIAL <input type="checkbox"/> FEDERAL <input type="checkbox"/> GENERAL <input checked="" type="checkbox"/> OTHER								
PHASE	TOTAL				PROJECTED CASH REQUIREMENTS FOR PLANNING PURPOSES ONLY				SIX YEAR TOTAL	BALANCE TO COMPLETE
	ESTIMATED COST (\$000)	EXPEND THRU 2010	CURRENT YEAR 2011	BUDGET YEAR 2012	.....2013.....	.....2014.....	.....2015.....	.....2016.....		
Planning	0	0	0	0	0	0	0	0	0	0
Engineering	4,984	4,984	0	0	0	0	0	0	0	0
Right-of-way	6,525	4,749	1,776	0	0	0	0	0	1,776	0
Construction	4,440	732	2,602	1,106	0	0	0	0	3,708	0
Total	15,949	10,465	4,378	1,106	0	0	0	0	5,484	0
Federal-Aid	0	0	0	0	0	0	0	0	0	0

SIGNIFICANT CHANGE FROM FY 2010 - 15 CTP: Cost reduced by \$2.1 million based on actual bid prices.



PROJECT: Francis Scott Key Bridge - Police Outdoor Firing Range

DESCRIPTION: Construct new Police Outdoor Firing Range at Hawkins Point for the Maryland Transportation Authority (MDTA) Police.

PURPOSE & NEED SUMMARY STATEMENT: The project will provide additional capacity and improve training conditions for the MDTA Police and other State and local police personnel.

STATE GOALS : Maryland Transportation Plan (MTP) Goals/Selection Criteria:

- Quality of Service
- System Preservation & Performance
- Connectivity for Daily Life
- Safety & Security
- Environmental Stewardship

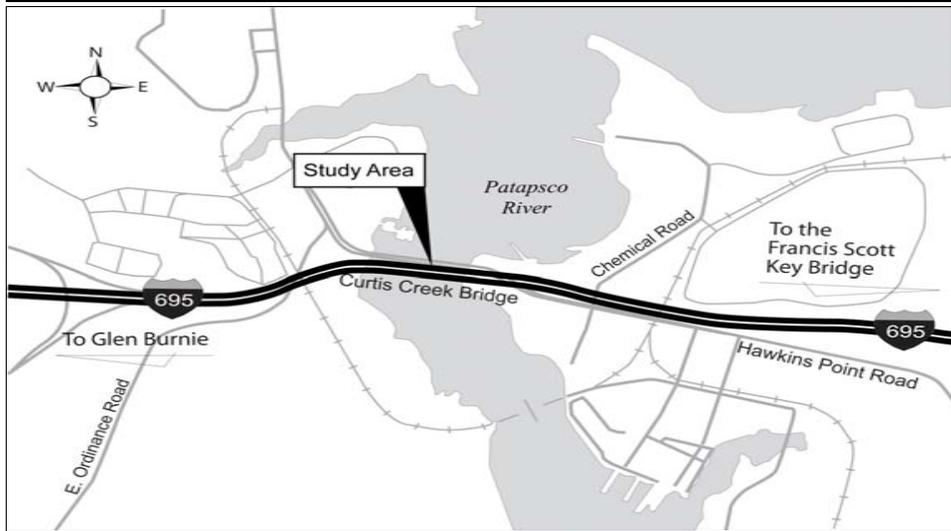
ASSOCIATED IMPROVEMENTS:
None.

EXPLANATION: The project will provide additional capacity and improve training conditions for the MDTA Police and other State and local police personnel.

STATUS: Project on hold pending an assessment of operations at the Sykesville training facility.

POTENTIAL FUNDING SOURCE:		<input type="checkbox"/> SPECIAL		<input type="checkbox"/> FEDERAL		<input type="checkbox"/> GENERAL		<input checked="" type="checkbox"/> OTHER			
PHASE	TOTAL		CURRENT YEAR	BUDGET YEAR	PROJECTED CASH REQUIREMENTS FOR PLANNING PURPOSES ONLY				SIX YEAR TOTAL	BALANCE TO COMPLETE	
	ESTIMATED COST (\$000)	EXPEND THRU 2010			2011	20122013....2014....		2015....	....2016....
Planning	27	27	0	0	0	0	0	0	0	0	
Engineering	1,603	1,384	0	219	0	0	0	0	219	0	
Right-of-way	0	0	0	0	0	0	0	0	0	0	
Construction	2,776	226	0	0	1,542	1,008	0	0	2,550	0	
Total	4,406	1,637	0	219	1,542	1,008	0	0	2,769	0	
Federal-Aid	0	0	0	0	0	0	0	0	0	0	

SIGNIFICANT CHANGE FROM FY 2010 - 15 CTP:
Construction start delayed to FY 2013.



PROJECT: MD 695 Francis Scott Key Bridge - Replace Curtis Creek Grid Deck

DESCRIPTION: Replace the steel grid deck of the bascule (movable) span, and retrofit stringers and floor beams.

PURPOSE & NEED SUMMARY STATEMENT: The grid deck is reaching the end of its useful life and stringers and floor beams exhibit various degrees of wear. This improvement will address existing wear and extend the useful life of the bridge.

STATE GOALS: Maryland Transportation Plan (MTP) Goals/Selection Criteria:

- Quality of Service
- System Preservation & Performance
- Connectivity for Daily Life
- Safety & Security
- Environmental Stewardship

EXPLANATION: This improvement will address existing wear and extend the useful life of the bridge.

ASSOCIATED IMPROVEMENTS:

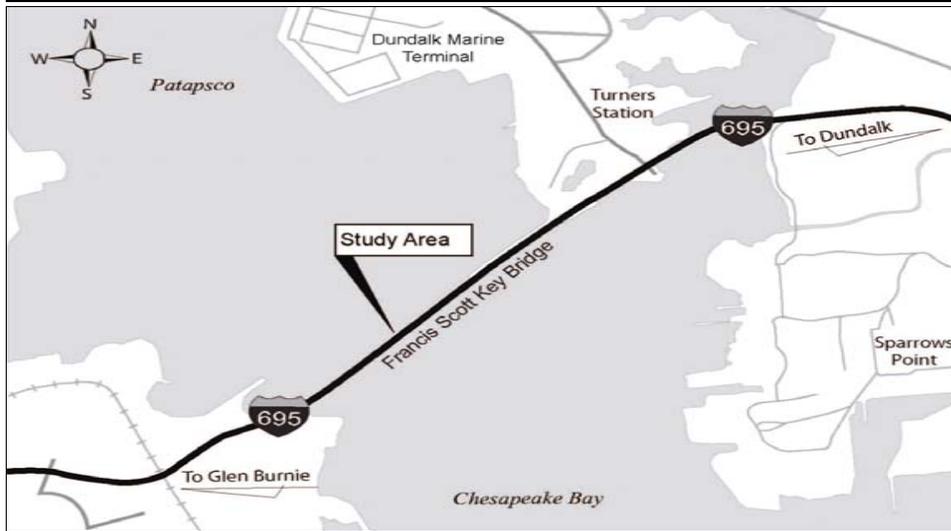
- MD 695 Francis Scott Key Bridge - Interchange Improvements at Quarantine Road - Construction Program (Line 12)
- MD 695 Francis Scott Key Bridge - Repair Approach Spans and Paint Fascia Beams - Construction Program (Line 15)

STATUS: Engineering is underway. Construction to begin in FY 2012.

POTENTIAL FUNDING SOURCE:		<input type="checkbox"/> SPECIAL		<input type="checkbox"/> FEDERAL		<input type="checkbox"/> GENERAL		<input checked="" type="checkbox"/> OTHER		
PHASE	TOTAL ESTIMATED COST (\$000)	EXPEND THRU 2010	CURRENT YEAR 2011	BUDGET YEAR 2012	PROJECTED CASH REQUIREMENTS FOR PLANNING PURPOSES ONLY				SIX YEAR TOTAL	BALANCE TO COMPLETE
					.....2013.....	.....2014.....	.....2015.....	.....2016.....		
Planning	0	0	0	0	0	0	0	0	0	0
Engineering	1,500	424	1,076	0	0	0	0	0	1,076	0
Right-of-way	0	0	0	0	0	0	0	0	0	0
Construction	12,947	0	0	12,947	0	0	0	0	12,947	0
Total	14,447	424	1,076	12,947	0	0	0	0	14,023	0
Federal-Aid	0	0	0	0	0	0	0	0	0	0

SIGNIFICANT CHANGE FROM FY 2010 - 15 CTP:

Construction start advanced two years to FY 2012 to follow recently completed emergency repair work.



PROJECT: MD 695 Francis Scott Key Bridge - Repair Approach Spans and Paint Bridge Fascia Beams

DESCRIPTION: Repair beams on the approach spans and paint the bridge fascia beams.

PURPOSE & NEED SUMMARY STATEMENT: Approach spans and fascia beams exhibit various degrees of wear. This improvement will address existing deterioration and prevent further deterioration.

STATE GOALS: Maryland Transportation Plan (MTP) Goals/Selection Criteria:

- Quality of Service
- System Preservation & Performance
- Connectivity for Daily Life
- Safety & Security
- Environmental Stewardship

EXPLANATION: Approach spans and fascia beams exhibit various degrees of wear. This improvement will address existing deterioration and prevent further deterioration.

ASSOCIATED IMPROVEMENTS:

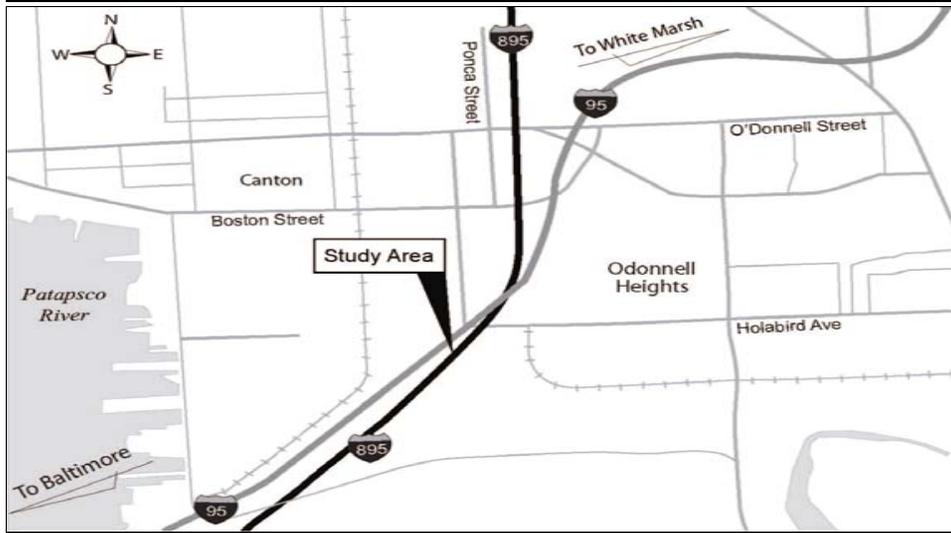
- MD 695 Francis Scott Key Bridge - Interchange Improvements at Quarantine Road - Construction Program (Line 12)
- MD 695 Francis Scott Key Bridge - Replace Curtis Creek Grid Deck - Construction Program (Line 14)

STATUS: Engineering is underway. Construction to begin in FY 2012.

PHASE	POTENTIAL FUNDING SOURCE:									
	<input type="checkbox"/> SPECIAL <input type="checkbox"/> FEDERAL <input type="checkbox"/> GENERAL <input checked="" type="checkbox"/> OTHER									
	TOTAL ESTIMATED COST (\$000)	TOTAL EXPEND THRU 2010	CURRENT YEAR 2011	BUDGET YEAR 2012	PROJECTED CASH REQUIREMENTS FOR PLANNING PURPOSES ONLY				SIX YEAR TOTAL	BALANCE TO COMPLETE
				.....2013.....	.....2014.....	.....2015.....	.....2016.....			
Planning	0	0	0	0	0	0	0	0	0	0
Engineering	401	33	183	123	62	0	0	0	368	0
Right-of-way	0	0	0	0	0	0	0	0	0	0
Construction	14,156	0	0	5,632	8,067	457	0	0	14,156	0
Total	14,557	33	183	5,755	8,129	457	0	0	14,524	0
Federal-Aid	0	0	0	0	0	0	0	0	0	0

SIGNIFICANT CHANGE FROM FY 2010 - 15 CTP:

Construction start advanced one year to FY 2012 based on findings of recent inspection report.



PROJECT: I-895 Baltimore Harbor Tunnel Thruway - Canton Viaduct Replacement

DESCRIPTION: Replace the Canton Viaduct which is the elevated portion of the roadway from the north portal entrance to Holabird Avenue.

PURPOSE & NEED SUMMARY STATEMENT: The bridge deck on this structure has not been renovated since 1985. Testing has indicated that portions of the viaduct are nearing the end of their life cycle. The bridge deck, substructure and superstructure are experiencing various degrees of wear. The Canton Viaduct is one of the MDTA's Structurally Deficient bridges.

STATE GOALS : Maryland Transportation Plan (MTP) Goals/Selection Criteria:

- Quality of Service
- System Preservation & Performance
- Connectivity for Daily Life
- Safety & Security
- Environmental Stewardship

ASSOCIATED IMPROVEMENTS:

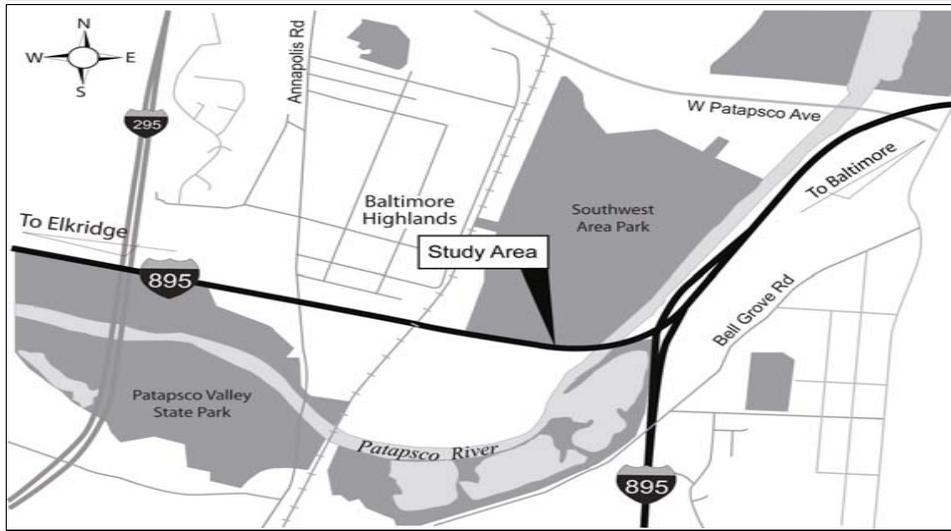
I-895 Baltimore Harbor Tunnel Thruway - Rehabilitate Patapsco Flats Bridge - Construction Program (Line 17)

EXPLANATION: Testing has indicated that portions of the viaduct are nearing the end of their life cycle.

STATUS: Engineering is underway. Right-of-way acquisition to begin in FY 2013. Construction to begin in FY 2015.

SIGNIFICANT CHANGE FROM FY 2010 - 15 CTP: None.

POTENTIAL FUNDING SOURCE:		<input type="checkbox"/> SPECIAL		<input type="checkbox"/> FEDERAL		<input type="checkbox"/> GENERAL		<input checked="" type="checkbox"/> OTHER			
PHASE	TOTAL		CURRENT YEAR	BUDGET YEAR	PROJECTED CASH REQUIREMENTS FOR PLANNING PURPOSES ONLY				SIX YEAR TOTAL	BALANCE TO COMPLETE	
	ESTIMATED COST (\$000)	EXPEND THRU 2010			2011	20122013....2014....		2015....	....2016....
Planning	0	0	0	0	0	0	0	0	0	0	
Engineering	5,137	2,586	601	815	815	320	0	0	2,551	0	
Right-of-way	10,920	0	0	0	5,460	5,460	0	0	10,920	0	
Construction	162,711	0	0	0	0	0	24,585	34,811	59,396	103,315	
Total	178,768	2,586	601	815	6,275	5,780	24,585	34,811	72,867	103,315	
Federal-Aid	0	0	0	0	0	0	0	0	0	0	



PROJECT: I-895 Baltimore Harbor Tunnel Thruway - Rehabilitate Patapsco Flats Bridge Substructure

DESCRIPTION: Repair worn concrete of the pier bearing pedestals, caps, and columns.

PURPOSE & NEED SUMMARY STATEMENT: Pier bearing pedestals, caps, and columns exhibit various degrees of wear. This improvement will address existing wear and prevent further wear.

STATE GOALS : Maryland Transportation Plan (MTP) Goals/Selection Criteria:

- Quality of Service
- System Preservation & Performance
- Connectivity for Daily Life
- Safety & Security
- Environmental Stewardship

ASSOCIATED IMPROVEMENTS:

I-895 Baltimore Harbor Tunnel Thruway - Canton Viaduct Replacement - Construction Program (Line 16)

EXPLANATION: Pier bearing pedestals, caps, and columns exhibit various degrees of wear. This improvement will address existing wear and prevent further wear.

STATUS: Engineering is underway. Construction to begin in FY 2012.

SIGNIFICANT CHANGE FROM FY 2010 - 15 CTP: None.

POTENTIAL FUNDING SOURCE:										
<input type="checkbox"/> SPECIAL <input type="checkbox"/> FEDERAL <input type="checkbox"/> GENERAL <input checked="" type="checkbox"/> OTHER										
PHASE	TOTAL				PROJECTED CASH REQUIREMENTS				SIX YEAR TOTAL	BALANCE TO COMPLETE
	ESTIMATED COST (\$000)	EXPEND THRU 2010	CURRENT YEAR 2011	BUDGET YEAR 2012	FOR PLANNING PURPOSES ONLY					
					.....2013.....	.....2014.....	.....2015.....	.....2016.....		
Planning	0	0	0	0	0	0	0	0	0	0
Engineering	1,220	47	717	342	114	0	0	0	1,173	0
Right-of-way	0	0	0	0	0	0	0	0	0	0
Construction	12,922	0	0	5,286	7,636	0	0	0	12,922	0
Total	14,142	47	717	5,628	7,750	0	0	0	14,095	0
Federal-Aid	0	0	0	0	0	0	0	0	0	0



PROJECT: I-895 Baltimore Harbor Tunnel Thruway - Decks/Superstructures

DESCRIPTION: Redeck the Hanover Street, Potee Street and Patapsco Avenue bridges and repair identified structural defects.

PURPOSE & NEED SUMMARY STATEMENT: The bridge decks are reaching the end of their useful life and need to be replaced.

STATE GOALS : Maryland Transportation Plan (MTP) Goals/Selection Criteria:

- Quality of Service
- System Preservation & Performance
- Connectivity for Daily Life
- Safety & Security
- Environmental Stewardship

ASSOCIATED IMPROVEMENTS:

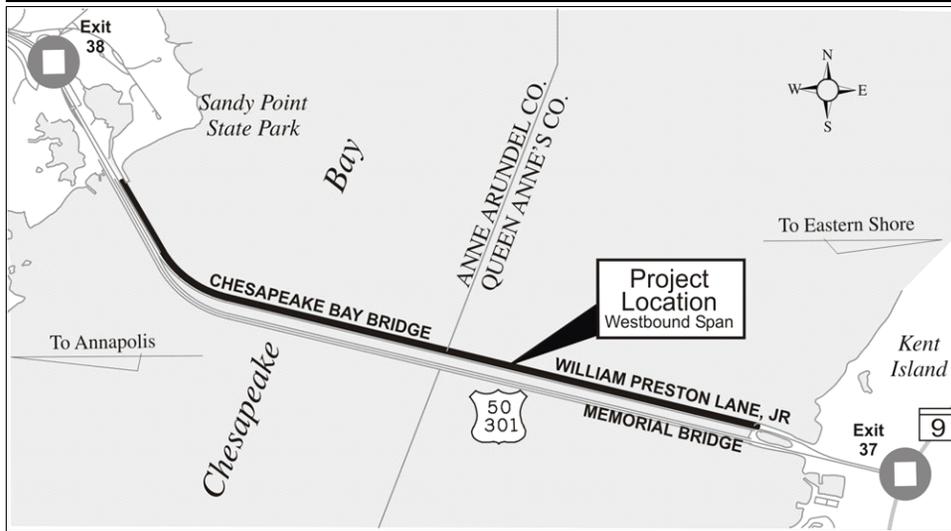
I-895 Baltimore Harbor Tunnel Thruway - Rehabilitate Patapsco Flats Bridge - Construction Program (Line 17)

EXPLANATION: The bridge decks are reaching the end of their useful life and need to be replaced.

STATUS: Engineering is underway. Construction to begin in FY 2013.

POTENTIAL FUNDING SOURCE:		<input type="checkbox"/> SPECIAL		<input type="checkbox"/> FEDERAL		<input type="checkbox"/> GENERAL		<input checked="" type="checkbox"/> OTHER			
PHASE	TOTAL		CURRENT YEAR	BUDGET YEAR	PROJECTED CASH REQUIREMENTS FOR PLANNING PURPOSES ONLY				SIX YEAR TOTAL	BALANCE TO COMPLETE	
	ESTIMATED COST (\$000)	EXPEND THRU 2010			2011	20122013.....2014.....		2015.....	.....2016.....
Planning	0	0	0	0	0	0	0	0	0	0	
Engineering	4,282	1	2,012	1,327	642	300	0	0	4,281	0	
Right-of-way	0	0	0	0	0	0	0	0	0	0	
Construction	18,930	0	0	0	9,590	9,340	0	0	18,930	0	
Total	23,212	1	2,012	1,327	10,232	9,640	0	0	23,211	0	
Federal-Aid	0	0	0	0	0	0	0	0	0	0	

SIGNIFICANT CHANGE FROM FY 2010 - 15 CTP: Project moved to Construction Program from System Preservation Minor Projects Program. Added construction phase.



PROJECT: US 50/301 Bay Bridge - Westbound Bridge Deck Rehabilitation

DESCRIPTION: Rehabilitate the westbound bridge deck. Phase I - Truss, Beam and Girder Spans. Phase II - Suspension and Through-truss Spans.

PURPOSE & NEED SUMMARY STATEMENT: The bridge deck on this structure has not been renovated since its opening in 1973. The bridge deck is exhibiting various degrees of wear and nearing the end of its useful life.

STATE GOALS : Maryland Transportation Plan (MTP) Goals/Selection Criteria:

- Quality of Service
- System Preservation & Performance
- Connectivity for Daily Life
- Safety & Security
- Environmental Stewardship

EXPLANATION: The bridge deck is exhibiting various degrees of wear as it is nearing the end of its life cycle.

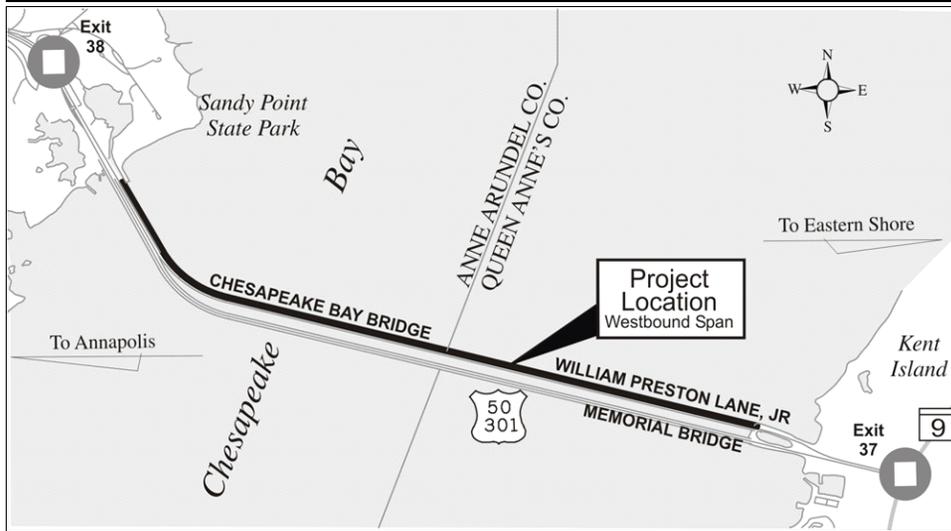
ASSOCIATED IMPROVEMENTS:

- US 50/301 Bay Bridge - Cleaning and Painting Structural Steel Westbound Bay Bridge - Construction Program (Line 20)
- US 50/301 Bay Bridge - Cable Rewrapping & Dehumidification - Construction Program (Line 21)
- US 50/301 Bay Bridge - Repair Underwater Scour & Pier Concrete - Construction Program (Line 22)

STATUS: Phase I construction is complete. Phase II construction is nearing completion.

POTENTIAL FUNDING SOURCE:		<input type="checkbox"/> SPECIAL		<input type="checkbox"/> FEDERAL		<input type="checkbox"/> GENERAL		<input checked="" type="checkbox"/> OTHER			
PHASE	TOTAL		CURRENT YEAR	BUDGET YEAR	PROJECTED CASH REQUIREMENTS FOR PLANNING PURPOSES ONLY				SIX YEAR TOTAL	BALANCE TO COMPLETE	
	ESTIMATED COST (\$000)	EXPEND THRU 2010			2011	20122013.....2014.....		2015.....	.....2016.....
Planning	0	0	0	0	0	0	0	0	0	0	0
Engineering	15,005	14,961	44	0	0	0	0	0	0	44	0
Right-of-way	0	0	0	0	0	0	0	0	0	0	0
Construction	119,239	91,077	28,162	0	0	0	0	0	0	28,162	0
Total	134,244	106,038	28,206	0	0	0	0	0	0	28,206	0
Federal-Aid	0	0	0	0	0	0	0	0	0	0	0

SIGNIFICANT CHANGE FROM FY 2010 - 15 CTP: Cost increased by \$2.2 million due to adjustment in previous years payroll allocations.



PROJECT: US 50/301 Bay Bridge - Cleaning and Painting Structural Steel Westbound Bridge

DESCRIPTION: Partial painting of all structural steel surfaces on the westbound bridge of approximately 3 million square feet including girder spans, deck trusses, overhead truss members and suspension spans.

PURPOSE & NEED SUMMARY STATEMENT: The bridge has not been fully painted since it opened in 1973. Limited spot painting has been done as needed. The paint is showing signs of wear. The improvement will protect the steel components of the bridge and extend the useful life.

STATE GOALS: Maryland Transportation Plan (MTP) Goals/Selection Criteria:

- Quality of Service
- System Preservation & Performance
- Connectivity for Daily Life
- Safety & Security
- Environmental Stewardship

EXPLANATION: The paint is showing signs of wear. This improvement will address existing wear and extend the useful life of the steel components of the bridge.

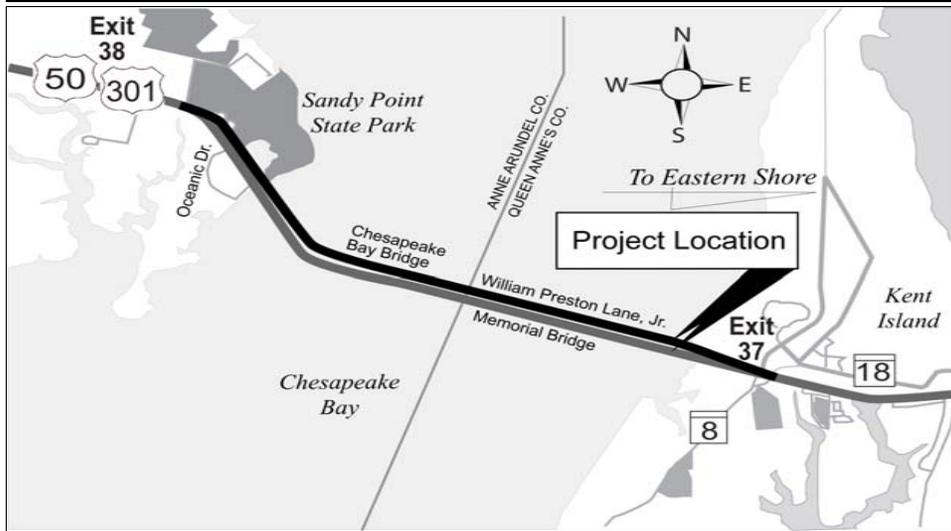
ASSOCIATED IMPROVEMENTS:

- US 50/301 Bay Bridge - Westbound Deck Rehabilitation - Construction Program (Line 19)
- US 50/301 Bay Bridge - Cable Rewrapping & Dehumidification - Construction Program (Line 21)
- US 50/301 Bay Bridge - Repair Underwater Scour & Pier Concrete - Construction Program (Line 22)

STATUS: Engineering is underway. Construction to begin in FY 2011.

SIGNIFICANT CHANGE FROM FY 2010 - 15 CTP: None.

POTENTIAL FUNDING SOURCE:		<input type="checkbox"/> SPECIAL		<input type="checkbox"/> FEDERAL		<input type="checkbox"/> GENERAL		<input checked="" type="checkbox"/> OTHER			
PHASE	TOTAL		CURRENT YEAR	BUDGET YEAR	PROJECTED CASH REQUIREMENTS FOR PLANNING PURPOSES ONLY				SIX YEAR TOTAL	BALANCE TO COMPLETE	
	ESTIMATED COST (\$000)	EXPEND THRU 2010			2011	20122013.....2014.....		2015.....	.....2016.....
Planning	0	0	0	0	0	0	0	0	0	0	
Engineering	2,506	183	895	828	600	0	0	0	2,323	0	
Right-of-way	0	0	0	0	0	0	0	0	0	0	
Construction	35,155	0	1,409	13,564	20,182	0	0	0	35,155	0	
Total	37,661	183	2,304	14,392	20,782	0	0	0	37,478	0	
Federal-Aid	0	0	0	0	0	0	0	0	0	0	



PROJECT: US 50/301 Bay Bridge - Cable Rewrapping & Dehumidification

DESCRIPTION: Rewrap, dehumidify and shield the main cables and anchorages on both bridges.

PURPOSE & NEED SUMMARY STATEMENT: The existing suspension cable wrapping is reaching the end of its useful life. This improvement will prevent cable corrosion and provide shielding for the main cables.

STATE GOALS: Maryland Transportation Plan (MTP) Goals/Selection Criteria:

- Quality of Service
- System Preservation & Performance
- Connectivity for Daily Life
- Safety & Security
- Environmental Stewardship

EXPLANATION: Cable wrapping is nearing the end of its useful life and needs to be replaced.

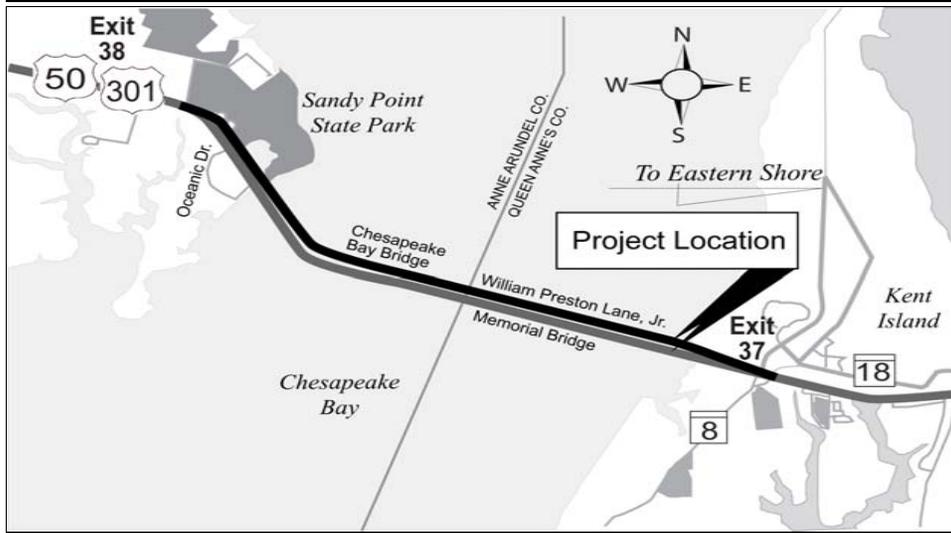
ASSOCIATED IMPROVEMENTS:

- US 50/301 Bay Bridge - Westbound Bridge Deck Rehabilitation - Construction Program (Line 19)
- US 50/301 Bay Bridge - Cleaning and Painting Structural Steel Westbound Bridge - Construction Program (Line 20)
- US 50/301 Bay Bridge - Repair Underwater Scour & Pier Concrete - Construction Program (Line 22)

STATUS: Engineering is underway. Construction to begin in FY 2012.

POTENTIAL FUNDING SOURCE:										
<input type="checkbox"/> SPECIAL <input type="checkbox"/> FEDERAL <input type="checkbox"/> GENERAL <input checked="" type="checkbox"/> OTHER										
PHASE	TOTAL		CURRENT YEAR	BUDGET YEAR	PROJECTED CASH REQUIREMENTS FOR PLANNING PURPOSES ONLY				SIX YEAR TOTAL	BALANCE TO COMPLETE
	ESTIMATED COST (\$000)	EXPEND THRU 2010			2011	20122013.....2014.....			.....2015.....	.....2016.....
Planning	0	0	0	0	0	0	0	0	0	0
Engineering	2,303	448	1,355	500	0	0	0	0	1,855	0
Right-of-way	0	0	0	0	0	0	0	0	0	0
Construction	36,566	1	0	2,357	18,245	15,963	0	0	36,565	0
Total	38,869	449	1,355	2,857	18,245	15,963	0	0	38,420	0
Federal-Aid	0	0	0	0	0	0	0	0	0	0

SIGNIFICANT CHANGE FROM FY 2010 - 15 CTP: Project moved to Construction Program from System Preservation Minor Projects Program. Added construction phase.



PROJECT: US 50/301 Bay Bridge - Repair Underwater Scour and Pier Concrete - Phase 1

DESCRIPTION: System preservation work consisting of repairing wear to concrete piers and piles (above and below water) and replacing missing and damaged pile jackets.

PURPOSE & NEED SUMMARY STATEMENT: Piers and piles exhibit various degrees of wear. This improvement will address existing wear and extend the useful life of the piers.

STATE GOALS: Maryland Transportation Plan (MTP) Goals/Selection Criteria:

- Quality of Service
- System Preservation & Performance
- Connectivity for Daily Life
- Safety & Security
- Environmental Stewardship

EXPLANATION: Repair wear to concrete piers and piles and replace missing and damaged pile jackets.

ASSOCIATED IMPROVEMENTS:

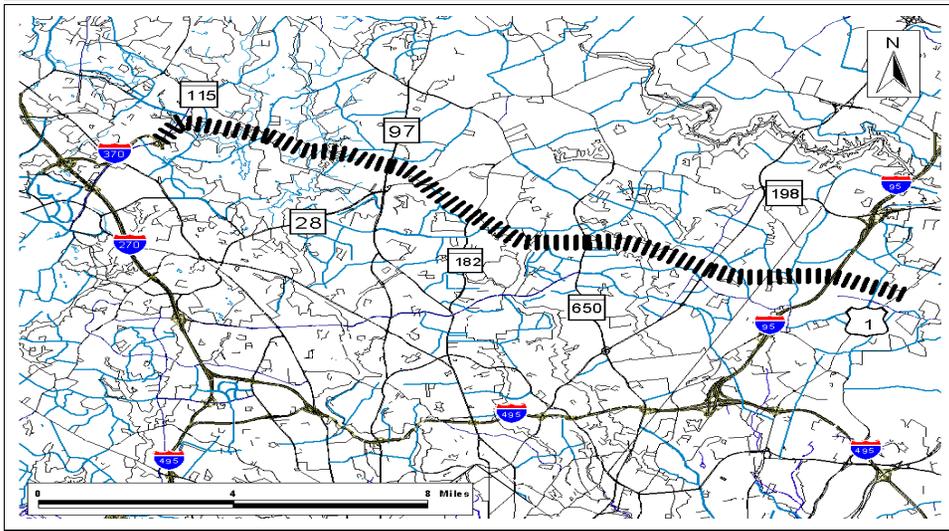
- US 50/301 Bay Bridge - Westbound Bridge Deck Rehabilitation - Construction Program (Line 19)
- US 50/301 Bay Bridge - Cleaning and Painting Structural Steel Westbound Bridge - Construction Program (Line 20)
- US 50/301 Bay Bridge - Cable Rewrapping & Dehumidification - Construction Program (Line 21)

STATUS: Engineering is underway. Construction to begin in FY 2012.

PHASE	TOTAL ESTIMATED COST (\$000)		CURRENT YEAR	BUDGET YEAR	PROJECTED CASH REQUIREMENTS FOR PLANNING PURPOSES ONLY				SIX YEAR TOTAL	BALANCE TO COMPLETE
	ESTIMATED COST (\$000)	EXPEND THRU 2010			2011	20122013.....2014.....			.....2015.....	.....2016.....
Planning	0	0	0	0	0	0	0	0	0	0
Engineering	2,000	95	823	464	309	309	0	0	1,905	0
Right-of-way	0	0	0	0	0	0	0	0	0	0
Construction	12,551	0	0	3,108	7,650	1,793	0	0	12,551	0
Total	14,551	95	823	3,572	7,959	2,102	0	0	14,456	0
Federal-Aid	0	0	0	0	0	0	0	0	0	0

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SIGNIFICANT CHANGE FROM FY 2010 - 15 CTP: Project moved to Construction Program from System Preservation Minor Projects Program. Added construction phase.



PROJECT: MD 200, InterCounty Connector

DESCRIPTION: Construction of a new east-west, multi-modal highway in Montgomery and Prince George's counties between I-270 and I-95/US 1.

PURPOSE & NEED SUMMARY STATEMENT: This transportation project is needed to increase community mobility and safety; to support development and local land use plans; to improve access between economic growth centers; to advance homeland security measures; and to help restore the natural, human and cultural environments from past development impacts in the project area.

STATE GOALS : Maryland Transportation Plan (MTP) Goals/Selection Criteria:

- Quality of Service
- System Preservation & Performance
- Connectivity for Daily Life
- Safety & Security
- Environmental Stewardship

EXPLANATION: The project increases community mobility and safety; supports development and local land use plans; improves access between economic growth centers; advances homeland security measures; and helps restore the natural, human and cultural environments from past development impacts in the project area.

ASSOCIATED IMPROVEMENTS:

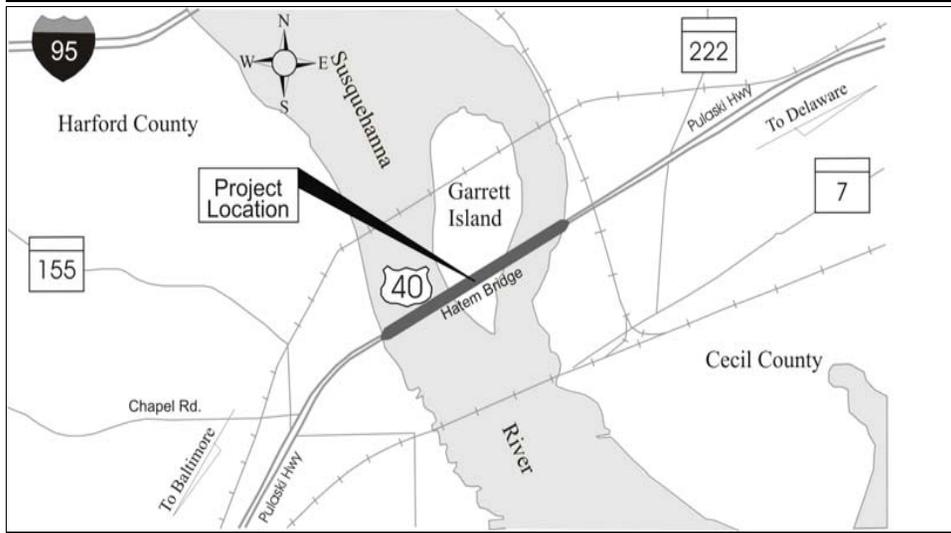
- I-95/Contee Road Interchange (Line 5, Montgomery County - SHA)
- MD 28/MD 198, MD 97 to I-95 (Line 19, Montgomery County - SHA)
- MD 201 Extended/US 1, I-95/I-495 to Contee Road (Line 21, Prince George's County - SHA)

STATUS: Engineering, right-of-way and construction are underway. The MDTA and SHA are reviewing potential adjustments to project scope, and other budget and funding mitigation measures to address the increased cost of contract B (east of MD 97 to west of US 29), which might allow all or a portion of the deferred work to be undertaken. Contract D is not reflected in the cashflow.

SIGNIFICANT CHANGE FROM FY 2010 - 15 CTP: The total project cost was reduced by \$103.2 million. This reflects the removal of the balance to complete budget from this project sheet. The balance to complete obligation is still reflected in MDTA financial forecast.

POTENTIAL FUNDING SOURCE:		<input checked="" type="checkbox"/> SPECIAL	<input checked="" type="checkbox"/> FEDERAL	<input checked="" type="checkbox"/> GENERAL	<input checked="" type="checkbox"/> OTHER						
PHASE	TOTAL		CURRENT YEAR	BUDGET YEAR	PROJECTED CASH REQUIREMENTS FOR PLANNING PURPOSES ONLY				SIX YEAR TOTAL	BALANCE TO COMPLETE	
	ESTIMATED COST (\$000)	EXPEND THRU 2010			2011	20122013.....2014.....		2015.....	.....2016.....
Planning	28,889	28,889	0	0	0	0	0	0	0	0	
Engineering	75,181	75,181	0	0	0	0	0	0	0	0	
Right-of-way	298,000	268,533	17,642	5,862	3,479	2,484	0	0	29,467	0	
Construction	2,060,660	1,075,476	626,965	284,980	39,173	34,066	0	0	985,184	0	
Total	2,462,730	1,448,079	644,607	290,842	42,652	36,550	0	0	1,014,651	0	
Federal-Aid	19,270	19,270	0	0	0	0	0	0	0	0	

The Federal aid share of \$19.27 million is also included in SHA's portion of the CTP.
1982



PROJECT: US 40 Thomas J. Hatem Memorial Bridge - Deck Replacement

DESCRIPTION: Replace the deck on the Hatem Bridge. One 12-foot wide traffic lane will be maintained in each direction during construction. This maintenance of traffic plan was selected through coordination efforts with the Cecil/Harford Bridges Work Group. (BRAC related.)

PURPOSE & NEED SUMMARY STATEMENT: The concrete and steel grate core are worn, requiring replacement. The deck was last renovated in 1982. This improvement will completely replace the bridge deck.

STATE GOALS : Maryland Transportation Plan (MTP) Goals/Selection Criteria:

- Quality of Service
- System Preservation & Performance
- Connectivity for Daily Life
- Safety & Security
- Environmental Stewardship

EXPLANATION: The concrete and steel grate core are worn and are reaching the end of their life cycle. This improvement will completely replace the deck with a new grid deck.

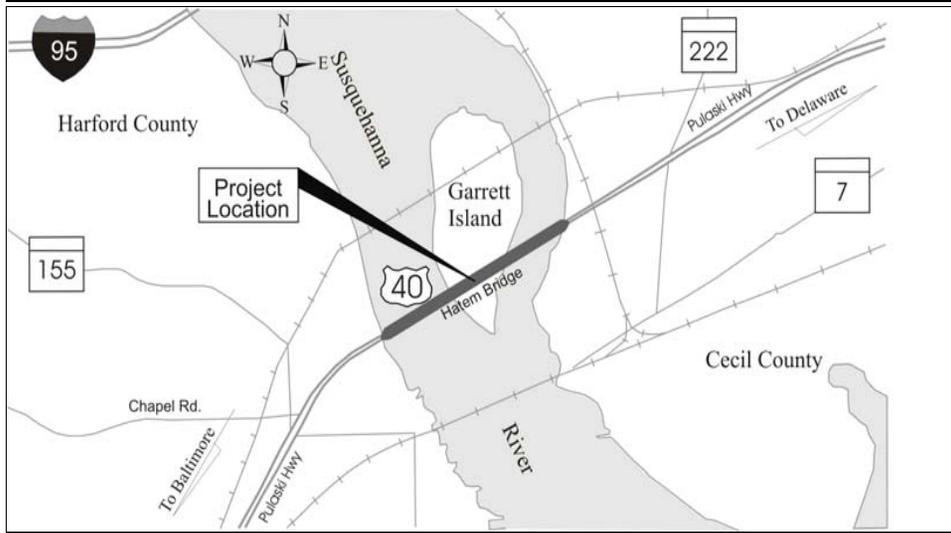
ASSOCIATED IMPROVEMENTS:

- US 40 Thomas J. Hatem Memorial Bridge - Underwater Repairs - Construction Program (Line 25)
- US 40 Thomas J. Hatem Memorial Bridge - Cleaning and Painting Structural Steel - Construction Program (Line 26)

STATUS: Construction is underway.

POTENTIAL FUNDING SOURCE:		<input type="checkbox"/> SPECIAL		<input type="checkbox"/> FEDERAL		<input type="checkbox"/> GENERAL		<input checked="" type="checkbox"/> OTHER			
PHASE	TOTAL		CURRENT YEAR	BUDGET YEAR	PROJECTED CASH REQUIREMENTS FOR PLANNING PURPOSES ONLY				SIX YEAR TOTAL	BALANCE TO COMPLETE	
	ESTIMATED COST (\$000)	EXPEND THRU 2010			2011	20122013....2014....		2015....	....2016....
Planning	0	0	0	0	0	0	0	0	0	0	0
Engineering	3,702	3,548	135	19	0	0	0	0	0	154	0
Right-of-way	0	0	0	0	0	0	0	0	0	0	0
Construction	63,065	48,847	13,422	796	0	0	0	0	0	14,218	0
Total	66,767	52,395	13,557	815	0	0	0	0	0	14,372	0
Federal-Aid	0	0	0	0	0	0	0	0	0	0	0

SIGNIFICANT CHANGE FROM FY 2010 - 15 CTP: Cost increased by \$1.8 million due to adjustment in previous years payroll allocations.



PROJECT: US 40 Thomas J. Hatem Memorial Bridge - Underwater Repairs

DESCRIPTION: Rehabilitate pier foundations with advanced wear. Work will also provide pier scour protection.

PURPOSE & NEED SUMMARY STATEMENT: Pier foundations in the river were found to have advanced wear and analysis shows scour protection is needed. This improvement will extend the useful life of the pier foundations.

STATE GOALS : Maryland Transportation Plan (MTP) Goals/Selection Criteria:

- Quality of Service
- System Preservation & Performance
- Connectivity for Daily Life
- Safety & Security
- Environmental Stewardship

EXPLANATION: Pier foundations have advanced wear and require scour protection. This improvement will address existing wear and prevent further wear.

ASSOCIATED IMPROVEMENTS:

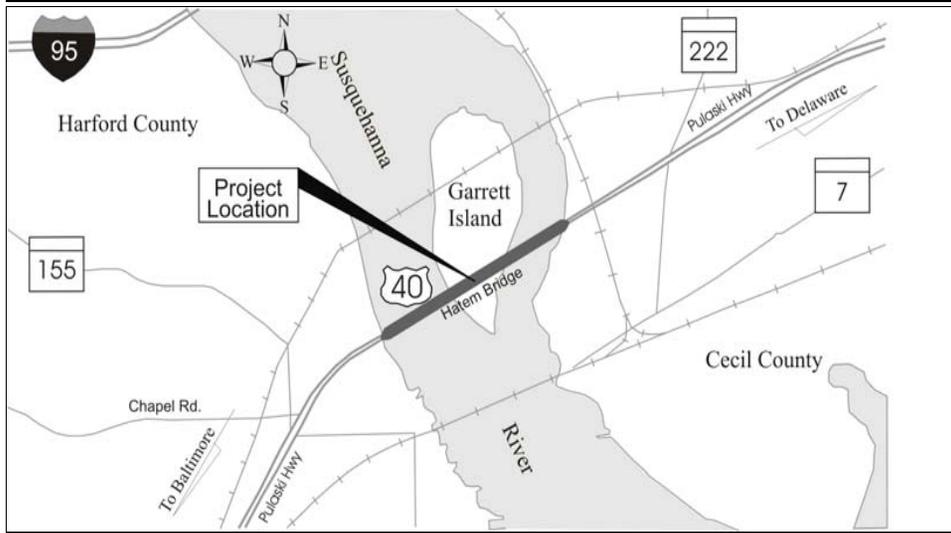
- US 40 Thomas J. Hatem Memorial Bridge - Deck Replacement - Construction Program (Line 24)
- US 40 Thomas J. Hatem Memorial Bridge - Cleaning and Painting Structural Steel - Construction Program (Line 26)

STATUS: Engineering is underway. Construction to begin in FY 2012.

PHASE	TOTAL		CURRENT YEAR	BUDGET YEAR	PROJECTED CASH REQUIREMENTS FOR PLANNING PURPOSES ONLY				SIX YEAR TOTAL	BALANCE TO COMPLETE
	ESTIMATED COST (\$000)	EXPEND THRU 2010			2011	20122013.....2014.....			.....2015.....	.....2016.....
Planning	0	0	0	0	0	0	0	0	0	0
Engineering	1,810	450	594	444	322	0	0	0	1,360	0
Right-of-way	0	0	0	0	0	0	0	0	0	0
Construction	52,514	0	0	13,129	39,385	0	0	0	52,514	0
Total	54,324	450	594	13,573	39,707	0	0	0	53,874	0
Federal-Aid	0	0	0	0	0	0	0	0	0	0

SIGNIFICANT CHANGE FROM FY 2010 - 15 CTP:

Construction start delayed one year to FY 2012 to coincide with completion of deck replacement. Cost increased by \$27.8 million for change in scope to include pier rehabilitation below the river bed.



PROJECT: US 40 Thomas J. Hatem Memorial Bridge - Cleaning and Painting Structural Steel

DESCRIPTION: Paint structural members throughout the bridge.

PURPOSE & NEED SUMMARY STATEMENT: Bridge has areas of significant paint wear. This improvement will address existing paint wear and extend the useful life of the structural steel.

STATE GOALS : Maryland Transportation Plan (MTP) Goals/Selection Criteria:

- Quality of Service
- System Preservation & Performance
- Connectivity for Daily Life
- Safety & Security
- Environmental Stewardship

ASSOCIATED IMPROVEMENTS:

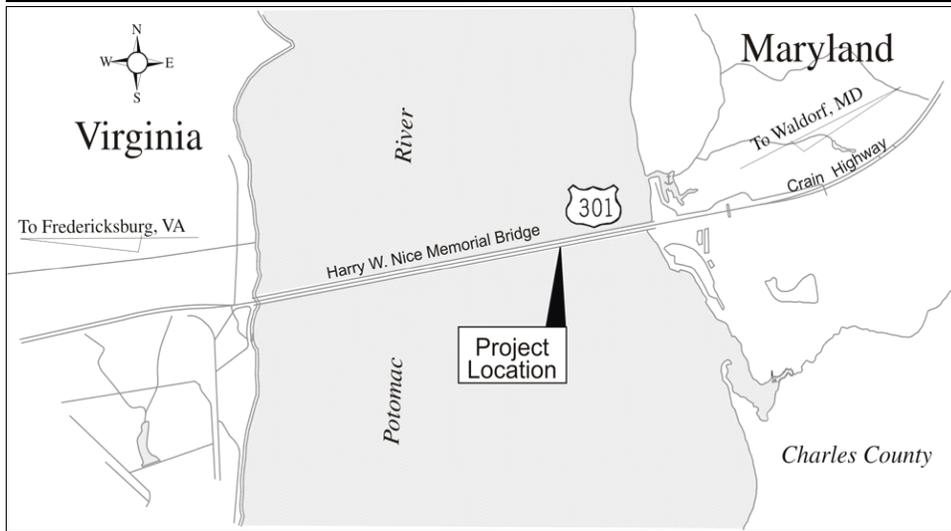
- US 40 Thomas J. Hatem Memorial - Bridge Deck Replacement - Construction Program (Line 24)
- US 40 Thomas J. Hatem Memorial - Bridge Underwater Repairs - Construction Program (Line 25)

EXPLANATION: Bridge has significant paint wear. This improvement will address existing wear and prevent further wear.

STATUS: Engineering is underway. Construction to begin in FY 2012.

POTENTIAL FUNDING SOURCE:		<input type="checkbox"/> SPECIAL <input type="checkbox"/> FEDERAL <input type="checkbox"/> GENERAL <input checked="" type="checkbox"/> OTHER								
PHASE	TOTAL				PROJECTED CASH REQUIREMENTS				SIX YEAR TOTAL	BALANCE TO COMPLETE
	ESTIMATED COST (\$000)	EXPEND THRU 2010	CURRENT YEAR 2011	BUDGET YEAR 2012	FOR PLANNING PURPOSES ONLY					
					.....2013.....	.....2014.....	.....2015.....	.....2016.....		
Planning	0	0	0	0	0	0	0	0	0	0
Engineering	500	27	473	0	0	0	0	0	473	0
Right-of-way	0	0	0	0	0	0	0	0	0	0
Construction	10,300	0	0	3,053	7,247	0	0	0	10,300	0
Total	10,800	27	473	3,053	7,247	0	0	0	10,773	0
Federal-Aid	0	0	0	0	0	0	0	0	0	0

SIGNIFICANT CHANGE FROM FY 2010 - 15 CTP: Cost increased by \$300 thousand for inflation.



PROJECT: US 301 Harry W. Nice Bridge - Cleaning and Painting Structural Steel and Miscellaneous Structural Repairs

DESCRIPTION: Spot paint as required throughout the full length of the bridge and zone paint at locations of roadway joints. Repair miscellaneous structural elements.

PURPOSE & NEED SUMMARY STATEMENT: Last zone paint completed was on the truss towers in 2002. Paint on many locations throughout the bridge has significant wear. This improvement will extend the useful life of the structural steel.

STATE GOALS : Maryland Transportation Plan (MTP) Goals/Selection Criteria:

- Quality of Service
- System Preservation & Performance
- Connectivity for Daily Life
- Safety & Security
- Environmental Stewardship

EXPLANATION: Paint throughout the bridge has significant wear. This improvement will address existing wear and prevent further wear.

ASSOCIATED IMPROVEMENTS:

US 301 Harry W. Nice Memorial Bridge - Approach Roadway Rehabilitation, Realignment, Toll Booth Replacement and Misc. Bridge Repairs - Construction Program (Line 28)

STATUS: Engineering is underway. Construction to begin in FY 2013.

PHASE	POTENTIAL FUNDING SOURCE:											
	TOTAL		CURRENT YEAR		BUDGET YEAR		PROJECTED CASH REQUIREMENTS FOR PLANNING PURPOSES ONLY				SIX YEAR TOTAL	BALANCE TO COMPLETE
	ESTIMATED COST (\$000)	EXPEND THRU 2010	YEAR 2011	YEAR 2012	....2013....	....2014....	....2015....	....2016....			
Planning	0	0	0	0	0	0	0	0	0	0	0	
Engineering	640	0	94	187	187	172	0	0	0	640	0	
Right-of-way	0	0	0	0	0	0	0	0	0	0	0	
Construction	9,360	0	0	0	3,744	5,499	117	0	0	9,360	0	
Total	10,000	0	94	187	3,931	5,671	117	0	0	10,000	0	
Federal-Aid	0	0	0	0	0	0	0	0	0	0	0	

SIGNIFICANT CHANGE FROM FY 2010 - 15 CTP: Cost decreased by \$11.8 million due to the elimination of fatigue retrofit work following a fatigue analysis study. Construction start delayed by one year to FY 2013 to coordinate with current bridge repair work.



PROJECT: US 301 Harry W. Nice Memorial Bridge - Approach Roadway Rehabilitation, Realignment, Toll Booth Replacement and Misc. Bridge Repairs

DESCRIPTION: Rehabilitate and realign the approach roadways, replace the existing toll booths and perform misc. structural repairs.

PURPOSE & NEED SUMMARY STATEMENT: The existing toll booths and approach roadways are nearing the end of their useful life. Minor bridge repair has also been identified as needed.

STATE GOALS : Maryland Transportation Plan (MTP) Goals/Selection Criteria:

- Quality of Service
- System Preservation & Performance
- Connectivity for Daily Life
- Safety & Security
- Environmental Stewardship

ASSOCIATED IMPROVEMENTS:

US 301 Harry W. Nice Memorial Bridge - Cleaning and Painting Structural Steel and Miscellaneous Structural Repairs - Construction Program (Line 27)

EXPLANATION: The existing toll booths and approach roadways are nearing the end of their useful life and need replacement. Misc. structural repairs will also be performed on the bridge.

STATUS: Engineering is complete. Construction underway.

PHASE	POTENTIAL FUNDING SOURCE:									
	<input type="checkbox"/> SPECIAL <input type="checkbox"/> FEDERAL <input type="checkbox"/> GENERAL <input checked="" type="checkbox"/> OTHER									
	TOTAL ESTIMATED COST (\$000)	EXPEND THRU 2010	CURRENT YEAR 2011	BUDGET YEAR 2012	PROJECTED CASH REQUIREMENTS FOR PLANNING PURPOSES ONLY				SIX YEAR TOTAL	BALANCE TO COMPLETE
				.....2013.....	.....2014.....	.....2015.....	.....2016.....			
Planning	0	0	0	0	0	0	0	0	0	0
Engineering	689	639	50	0	0	0	0	0	50	0
Right-of-way	0	0	0	0	0	0	0	0	0	0
Construction	10,185	7,984	2,201	0	0	0	0	0	2,201	0
Total	10,874	8,623	2,251	0	0	0	0	0	2,251	0
Federal-Aid	0	0	0	0	0	0	0	0	0	0

SIGNIFICANT CHANGE FROM FY 2010 - 15 CTP: Project moved to Construction Program from System Preservation Minor Projects Program. Construction cost greater than \$10 million.



PROJECT: Authority-wide - Upgrade MDTA Radio Communication Systems (700 MHz System)

DESCRIPTION: The State of Maryland intends to purchase an integrated statewide wireless communications system. The system will operate with the 700/800 MHz band frequencies. The implementation of the system will incorporate a phased deployment methodology. The Maryland Transportation Authority will be the first agency to work within this statewide project. MDTA is in Phase 1 Region 1-A which consists of BWI, ICC, Tunnel Command, Nice Bridge, Bay Bridge, and JFK North.

PURPOSE & NEED SUMMARY STATEMENT: The upgraded radio communications systems will provide state, local, and regional public first responders' real time operable and interoperable voice and data services that support Day-to-Day, Mutual Aid, and Task Force operations.

STATE GOALS : Maryland Transportation Plan (MTP) Goals/Selection Criteria:

- Quality of Service
- System Preservation & Performance
- Connectivity for Daily Life
- Safety & Security
- Environmental Stewardship

ASSOCIATED IMPROVEMENTS: None.

EXPLANATION: The upgraded radio communications systems will provide state, local, and regional public first responders' real time operable and interoperable voice and data services that support Day-to-Day, Mutual Aid, and Task Force operations.

STATUS: Maryland Board of Public Works approved contract on November 17, 2010. Notice to proceed issued December 15, 2010.

SIGNIFICANT CHANGE FROM FY 2010 - 15 CTP: None.

POTENTIAL FUNDING SOURCE:										
				<input type="checkbox"/> SPECIAL	<input type="checkbox"/> FEDERAL	<input type="checkbox"/> GENERAL	<input checked="" type="checkbox"/> OTHER			
PHASE	TOTAL		CURRENT YEAR	BUDGET YEAR	PROJECTED CASH REQUIREMENTS FOR PLANNING PURPOSES ONLY				SIX YEAR TOTAL	BALANCE TO COMPLETE
	ESTIMATED COST (\$000)	EXPEND THRU 2010			2011	20122013....2014....			....2015....	....2016....
Planning	0	0	0	0	0	0	0	0	0	0
Engineering	1,680	1,002	276	200	202	0	0	0	678	0
Right-of-way	0	0	0	0	0	0	0	0	0	0
Construction	22,000	0	2,000	10,000	10,000	0	0	0	22,000	0
Total	23,680	1,002	2,276	10,200	10,202	0	0	0	22,678	0
Federal-Aid	0	0	0	0	0	0	0	0	0	0



PROJECT: Authority-wide - Procure, Upgrade, Repair and Replace Signs Including Dynamic Message Signs and Sign Structures

DESCRIPTION: Upgrade and replace signs and sign structures and perform miscellaneous repairs. Also, supply dynamic message signs (DMS) for a 5 year period to support about 20+ projects that are replacing or installing new DMS throughout MDTA's facilities. The project also provides for preventive maintenance, emergency maintenance, and parts and supplies as needed to maintain the DMS signs.

PURPOSE & NEED SUMMARY STATEMENT: Existing DMS signs are experiencing reliability issues and other signs and sign structures are worn. This improvement will also bring signs and other safety features MDTA-wide up to latest standards.

STATE GOALS : Maryland Transportation Plan (MTP) Goals/Selection Criteria:

- Quality of Service
- System Preservation & Performance
- Connectivity for Daily Life
- Safety & Security
- Environmental Stewardship

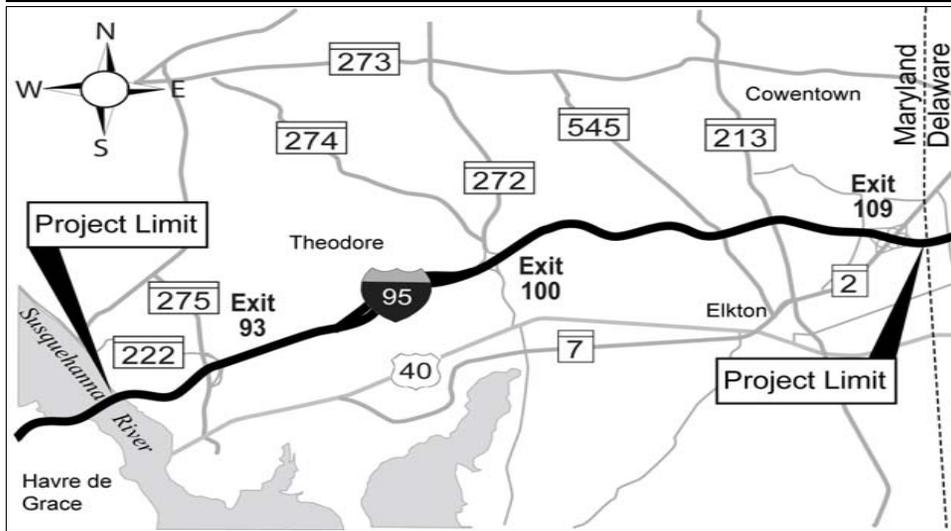
ASSOCIATED IMPROVEMENTS: None.

EXPLANATION: Existing signs, sign structures and DMS signs are worn, requiring upgrades and replacement.

STATUS: Construction is underway.

POTENTIAL FUNDING SOURCE:										
<input type="checkbox"/> SPECIAL <input type="checkbox"/> FEDERAL <input type="checkbox"/> GENERAL <input checked="" type="checkbox"/> OTHER										
PHASE	TOTAL		CURRENT YEAR	BUDGET YEAR	PROJECTED CASH REQUIREMENTS				SIX YEAR TOTAL	BALANCE TO COMPLETE
	ESTIMATED COST (\$000)	EXPEND THRU 2010			FOR PLANNING PURPOSES ONLY2013....2014....2015....			.....2016....
Planning	0	0	0	0	0	0	0	0	0	0
Engineering	4,068	2,966	1,102	0	0	0	0	0	1,102	0
Right-of-way	0	0	0	0	0	0	0	0	0	0
Construction	30,730	10,182	13,063	7,485	0	0	0	0	20,548	0
Total	34,798	13,148	14,165	7,485	0	0	0	0	21,650	0
Federal-Aid	0	0	0	0	0	0	0	0	0	0

SIGNIFICANT CHANGE FROM FY 2010 - 15 CTP: Cost decreased by \$4.6 million due to transfer of sign equipment to the ICC and ETL project budgets and less than anticipated expenditures.



PROJECT: I-95 John F. Kennedy Memorial Highway - Toll Plaza Planning Study

DESCRIPTION: Investigate alternative toll locations and collection methods. (BRAC related.)

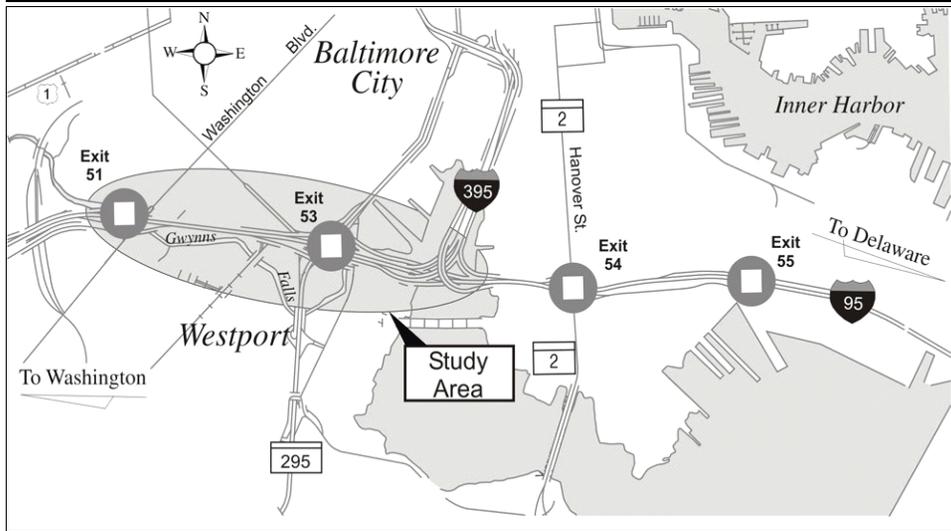
JUSTIFICATION: The study had three parts: 1) analyze travel patterns on I-95 and other major roads 2) evaluate potential tolling locations along I-95, including modifying the existing plaza, and 3) determine the best tolling method.

ASSOCIATED IMPROVEMENTS:
None.

STATUS: Planning is complete.

SIGNIFICANT CHANGE FROM FY 2010 - 15 CTP: None.

POTENTIAL FUNDING SOURCE:										
<input type="checkbox"/> SPECIAL <input type="checkbox"/> FEDERAL <input type="checkbox"/> GENERAL <input checked="" type="checkbox"/> OTHER										
PHASE	TOTAL				PROJECTED CASH REQUIREMENTS				SIX YEAR TOTAL	BALANCE TO COMPLETE
	ESTIMATED COST (\$000)	EXPEND THRU 2010	CURRENT YEAR 2011	BUDGET YEAR 2012	FOR PLANNING PURPOSES ONLY					
					.....2013.....	.....2014.....	.....2015.....	.....2016.....		
Planning	570	570	0	0	0	0	0	0	0	0
Engineering	0	0	0	0	0	0	0	0	0	0
Right-of-way	0	0	0	0	0	0	0	0	0	0
Construction	0	0	0	0	0	0	0	0	0	0
Total	570	570	0	0	0	0	0	0	0	0
Federal-Aid	0	0	0	0	0	0	0	0	0	0



PROJECT: I-95 Fort McHenry Tunnel - Carroll Camden Access Study

DESCRIPTION: Study to improve access to the Carroll Camden development area and improve safety and operations along I-95 between Washington Boulevard and I-395.

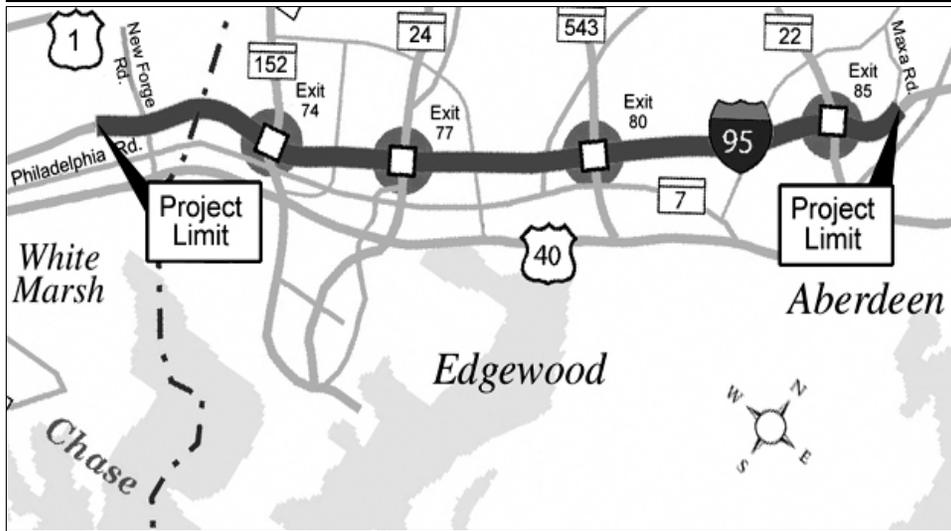
JUSTIFICATION: Improved access could help facilitate the redevelopment of the Carroll Camden area in Baltimore City and improve safety and operations along I-95.

ASSOCIATED IMPROVEMENTS: None.

STATUS: Feasibility study completed in June 2004. Interstate Access Point Approval (IAPA) study results show a negative impact on I-95 operations if additional interchange ramps are added. Waiting for Traffic Impact Study for the proposed casino in the City.

SIGNIFICANT CHANGE FROM FY 2010 - 15 CTP: None.

POTENTIAL FUNDING SOURCE:											
<input type="checkbox"/> SPECIAL <input type="checkbox"/> FEDERAL <input type="checkbox"/> GENERAL <input checked="" type="checkbox"/> OTHER											
PHASE	TOTAL				PROJECTED CASH REQUIREMENTS				SIX YEAR TOTAL	BALANCE TO COMPLETE	
	ESTIMATED COST (\$000)	EXPEND THRU 2010	CURRENT YEAR 2011	BUDGET YEAR 2012	FOR PLANNING PURPOSES ONLY	FOR PLANNING PURPOSES ONLY	FOR PLANNING PURPOSES ONLY	FOR PLANNING PURPOSES ONLY			
Planning	563	542	21	0	0	0	0	0	21	0	
Engineering	0	0	0	0	0	0	0	0	0	0	
Right-of-way	0	0	0	0	0	0	0	0	0	0	
Construction	0	0	0	0	0	0	0	0	0	0	
Total	563	542	21	0	0	0	0	0	21	0	
Federal-Aid	0	0	0	0	0	0	0	0	0	0	



PROJECT: I-95 John F. Kennedy Memorial Highway - Section 200 Study

DESCRIPTION: Investigate capacity and safety needs on I-95 from north of MD 43 to north of MD 22 (18 miles) including new park and ride facilities at I-95/MD 152 and I-95/MD 24 interchanges. (BRAC related.)

JUSTIFICATION: South of MD 152, I-95 operates at Level of Service (LOS) E (marginal service, irregular flow with speed variations) during weekday peak hours. Elsewhere, it operates at LOS D (reduced speeds, some congestion) or better during weekday and weekend peak traffic periods. Without improvements, the LOS is expected to decrease by 2020, with some study sections operating at an undesirable LOS F (lowest level of efficiency, frequent slowing required) during weekday and weekend periods.

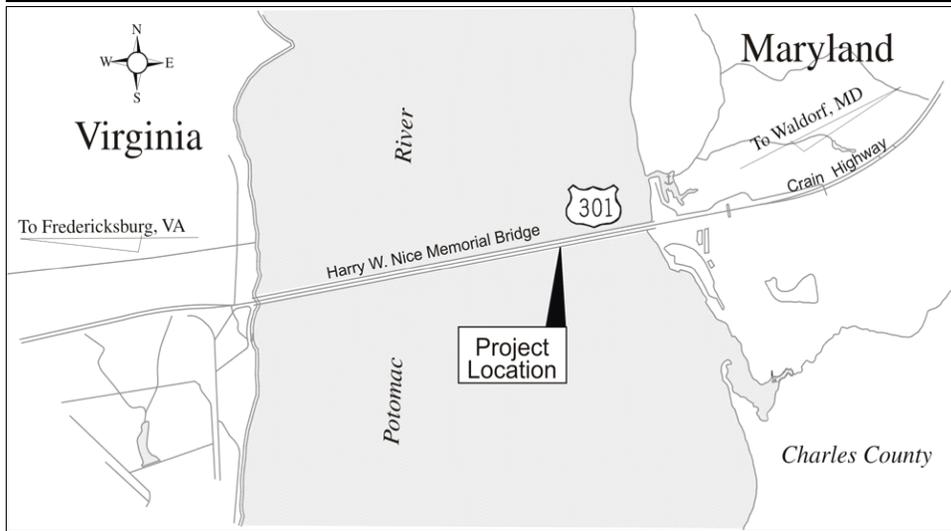
ASSOCIATED IMPROVEMENTS:

- I-95 Express Toll Lanes - Construction Program (Line 1)
- I-95 Interchange Improvements at MD 24 - Construction Program (Line 2)
- I-95 New Maintenance Facility (Line 4)

STATUS: Planning underway.

SIGNIFICANT CHANGE FROM FY 2010 - 15 CTP: Cost decreased by \$1.8 million due to reduction of right of way budget.

POTENTIAL FUNDING SOURCE:		<input type="checkbox"/> SPECIAL		<input type="checkbox"/> FEDERAL		<input type="checkbox"/> GENERAL		<input checked="" type="checkbox"/> OTHER		
PHASE	TOTAL				PROJECTED CASH REQUIREMENTS				SIX YEAR TOTAL	BALANCE TO COMPLETE
	ESTIMATED COST (\$000)	EXPEND THRU 2010	CURRENT YEAR 2011	BUDGET YEAR 2012	FOR PLANNING PURPOSES ONLY					
					.....2013.....	.....2014.....	.....2015.....	.....2016.....		
Planning	6,095	1,917	4,178	0	0	0	0	0	4,178	0
Engineering	4,647	4,647	0	0	0	0	0	0	0	0
Right-of-way	236	236	0	0	0	0	0	0	0	0
Construction	0	0	0	0	0	0	0	0	0	0
Total	10,978	6,800	4,178	0	0	0	0	0	4,178	0
Federal-Aid	0	0	0	0	0	0	0	0	0	0



PROJECT: US 301 Harry W. Nice Memorial Bridge - Improvement Study

DESCRIPTION: Investigate capacity and safety needs of the bridge and approaches.

JUSTIFICATION: The existing bridge does not meet current standards, i.e., narrow lanes, lack of shoulders, and steep grades with lack of truck climbing lanes. The existing bridge during peak periods is reaching capacity. Traffic is projected to increase by 45 percent on weekdays and 33 percent on weekends by 2025.

ASSOCIATED IMPROVEMENTS:
None.

STATUS: Planning is underway.

SIGNIFICANT CHANGE FROM FY 2010 - 15 CTP: None.

POTENTIAL FUNDING SOURCE:										
<input type="checkbox"/> SPECIAL <input type="checkbox"/> FEDERAL <input type="checkbox"/> GENERAL <input checked="" type="checkbox"/> OTHER										
PHASE	TOTAL				PROJECTED CASH REQUIREMENTS				SIX YEAR TOTAL	BALANCE TO COMPLETE
	ESTIMATED COST (\$000)	EXPEND THRU 2010	CURRENT YEAR 2011	BUDGET YEAR 2012	FOR PLANNING PURPOSES ONLY	FOR PLANNING PURPOSES ONLY	FOR PLANNING PURPOSES ONLY	FOR PLANNING PURPOSES ONLY		
					.....2013.....	.....2014.....	.....2015.....	.....2016.....		
Planning	6,551	4,756	1,795	0	0	0	0	0	1,795	0
Engineering	0	0	0	0	0	0	0	0	0	0
Right-of-way	0	0	0	0	0	0	0	0	0	0
Construction	0	0	0	0	0	0	0	0	0	0
Total	6,551	4,756	1,795	0	0	0	0	0	1,795	0
Federal-Aid	0	0	0	0	0	0	0	0	0	0

SYSTEM PRESERVATION MINOR PROJECTS PROGRAM

MARYLAND TRANSPORTATION AUTHORITY - LINE 35

ITEM NO.	DESCRIPTION AND IMPROVEMENT TYPE	TOTAL ESTIMATED COST (\$000's)	CONSTRUCTION START
<u>FY 2011 and Prior</u>			
<u>BALTIMORE HARBOR TUNNEL</u>			
1	Study High Voltage Cable Replacement Needs and Options (2107)	101	Complete
2	Rehabilitate Tunnel Lighting and Control System (0264)	2,482	Underway
3	Replace Electrical Vault (0265)	2,017	Underway
4	Study to Rehabilitate or Replace Tunnel Fire Line Gate Valves (2086)	96	Underway
5	Environmental Landscape Buffer Along I-895 Right Of Way (Greektown Landscaping) (2163)	18	Underway
6	Replace Water Line - Frankfurst Ave. to Fairfield Service Building (2034)	630	Underway
7	Replace Moravia Road Salt Barn Dome Roof (2139)	145	Underway
8	Deck Rehabilitation of K-Truss Bridge (Code 1 System Preservation) (2210)	5,148	Underway
9	I-895 Sound Barriers: Public Outreach, Geotechnical Investigation and Preliminary Design (2233)	390	Underway
10	Rehabilitation of I-895 between Toll Plaza and the K-Truss Bridge (2037)	2,650	Underway
11	Replace Overheight Detection System (2151)	544	Underway
12	Replace Roof and Repair HVAC of Fairfield Service Building (2036)	2,441	Underway
13	Replace Drainage Pipe in Fresh Air Duct (Code 1 System Preservation) (2213)	2,796	Spring, 2011
14	Replace Dynamic Message Signs & Lane Use Signals with LED Based Technology (0281)	1,491	Spring, 2011
<u>F.S.KEY BRIDGE</u>			
15	Upgrade and Replace Existing Signs (0474)	4,189	Complete
16	Improve Northeast Approach Maintenance Access Road (0479)	516	Complete
17	Emergency Repairs to the Operating Machinery at Curtis Creek Drawbridge (2161)	547	Complete
18	Emergency Repairs Curtis Creek Drawbridge (2187)	6,322	Complete
19	Renovate Engineering Building (2028)	1,097	Underway
20	Fatigue Retrofits at Curtis Creek - Engineering only (Code 2 System Preservation) (2214)	1,218	Underway
21	Reconstruction of Service Road #3 at FSK Facility (Engineering only) (2234)	775	Underway
22	Renovate Maintenance Garage Building (2024)	1,866	Underway
23	Replace Roof of Police Headquarters (0478)	732	Underway
24	Replace Roofs of Administrative & Maintenance Buildings (2020)	891	Underway
25	Replace Underground with Aboveground Storage Tanks - Engineering only (Code 2 System Preservation) (2202)	100	Underway

SYSTEM PRESERVATION MINOR PROJECTS PROGRAM

MARYLAND TRANSPORTATION AUTHORITY - LINE 35 (cont'd)

ITEM NO.	DESCRIPTION AND IMPROVEMENT TYPE	TOTAL ESTIMATED COST (\$000's)	CONSTRUCTION START
<u>FY 2011 and Prior (cont'd)</u>			
<u>F.S.KEY BRIDGE (cont'd)</u>			
26	Replacement Breaker and Repair Miscellaneous Electricals (Code 1 System Preservation) (2203)	284	Underway
27	Replacement of Light Poles on Francis Scott Key Bridge (2150)	700	Spring, 2011
<u>FORT MCHENRY TUNNEL</u>			
28	Miscellaneous Repairs to Air Ducts and Tile Work Below Tunnel Walkway (1468)	2,539	Complete
29	Replace Electrical Vault (1478)	845	Complete
30	Study of Ramp J from Hanover St. to I-95 Southbound (2169)	10	Complete
31	Study to Extend Ramp Barrier Wall/Install Fence at Exit 57 (O'Donnell Street)/I-95 SB (2093)	10	Complete
32	Central Utility Plant Upgrade (1477)	1,254	Underway
33	Major Repairs to Tunnel Fire Protection System (1401)	6,317	Underway
34	Replace and Rehabilitate Electrical Switchgear (1476)	3,871	Underway
35	Replace East and West Vent Building Roofs (1402)	2,794	Underway
36	East Vent Building HVAC Study (2231)	25	Underway
37	Repair Hanover Street Ramp (2157)	1,592	Underway
38	Replace Administration Building Generator (2138)	320	Underway
39	Replace Dynamic Message Signs and Lane Use Signals with LED Based Technology (1463)	2,253	Underway
<u>INTERCOUNTY CONNECTOR</u>			
40	Virtual Weigh Study (2230)	75	Underway
<u>KENNEDY HIGHWAY</u>			
41	Improve I-95 Perryville Weigh Station Signs (2143)	1,917	Complete
42	Monitor Ground Water Recovery System at Maryland House Sunoco Station (1264)	149	Underway
43	Plan Review & Field Monitoring Abingdon Plant Tunnel under I-95 (2180)	200	Underway
44	Replace 5KV Cable for Rt. 222 Interchange (2155)	320	Underway

SYSTEM PRESERVATION MINOR PROJECTS PROGRAM

MARYLAND TRANSPORTATION AUTHORITY - LINE 35 (cont'd)

ITEM NO.	DESCRIPTION AND IMPROVEMENT TYPE	TOTAL ESTIMATED COST (\$000's)	CONSTRUCTION START
<u>FY 2011 and Prior (cont'd)</u>			
<u>KENNEDY HIGHWAY (cont'd)</u>			
45	I-95/MD222 Concept Study (Planning only) (2237)	425	Underway
46	Inspect York Building Products Bridge over I-95 (2179)	75	Spring, 2011
47	Salt Barn at MD 7 and I-695 (Joint project with SHA) (2181)	1,000	Spring, 2011
<u>MULTI-AREA</u>			
48	Annual Inspection of Authority Facilities - FY05-FY09 (1986)	12,537	Complete
49	Install Closed Circuit TV Systems at FSK, HWN, TJH Facilities (1976)	6,900	Complete
50	Maintenance and Repair of Security System (Phase I) (2075)	118	Complete
51	Upgrade and Replace Metal Traffic Barrier and Attenuators (2011)	2,731	Complete
52	Emergency Light Poles Replacement and Bases at FMT and FSK (2066)	430	Complete
53	Engineering Study for Travel Time System (2110)	31	Complete
54	Management Review of Offices of Engineering and Construction Divisions (2153)	10	Complete
55	Rehabilitation of Static Scale Pit at Perryville Scale House (NB and SB) (2119)	12	Complete
56	Repair & Replace Raised Pavement Marking (RPM) Housing and Lenses (2085)	48	Complete
57	Study to Upgrade Fire Alarm System in Tunnels (2104)	15	Complete
58	On-Going Miscellaneous Upgrade/Replace Highway Signs, Pavement Markers & RPM (1980)	4,091	Underway
59	Install Closed Circuit TV Systems and Fiber Optic Spurs along I-95 (1974)	8,410	Underway
60	Study Toll Facility Cost Allocation (2002)	299	Underway
61	Upgrade Authority Operation Center at FMT and BHT (1954)	9,003	Underway
62	Building Security Improvements - Phase II (1983)	7,920	Underway
63	On-Call Miscellaneous Structural Repairs at Various Facilities (1900)	8,988	Underway
64	Radio Rebroadcast Systems in Fort McHenry and Harbor Tunnels (1963)	913	Underway
65	Security Related Plans, Evaluations and Upgrades (1467)	400	Underway
66	Electronic Toll Collection Replacement and Operating System - 3rd Gen. Engineering only (2147)	12,184	Underway
67	Information Technology Strategic Plan (1942)	4,030	Underway
68	Install Highway Advisory Radio Transmitters and Signs - JFK and WPL (2008)	1,049	Underway
69	International Electrical Testing Association (NETA) Electrical Testing - BHT and FMT (2125)	207	Underway

SYSTEM PRESERVATION MINOR PROJECTS PROGRAM

MARYLAND TRANSPORTATION AUTHORITY - LINE 35 (cont'd)

ITEM NO.	DESCRIPTION AND IMPROVEMENT TYPE	TOTAL ESTIMATED COST (\$000's)	CONSTRUCTION START
	<u>FY 2011 and Prior (cont'd)</u>		
	<u>MULTI-AREA (cont'd)</u>		
70	Law Enforcement IT Systems Plan - Multi-Area (2060)	9,375	Underway
71	Misc. ITS and Electrical Engineering Studies (2129)	498	Underway
72	Miscellaneous Real Estate Services (1997)	4,439	Underway
73	Miscellaneous Traffic Studies (2131)	4,689	Underway
74	NPDES Environmental Compliance Inspection and Remediation (2007)	20,263	Underway
75	Planning Studies for Strategic Development - Second Generation (2001)	8,904	Underway
76	Study FMT/BHT Campus Master Plans (2044)	176	Underway
77	University of Maryland- Concept of Operations for Congestion Management (2133)	90	Underway
78	Annual Inspection of Authority Facilities (2084)	67,209	Underway
79	Cleaning and Painting, Bearing Repairs, Joint Modifications & Misc. Repairs at BHT and FSK Bridges (1940)	8,594	Underway
80	Comprehensive Building Inspection Program (Engineering only) (2177)	1,000	Underway
81	Conduct Authority Market Research (2170)	200	Underway
82	Furnish and Install Camera and Coding & Decoding Equipment - Codec and Related Equipment (2014)	4,287	Underway
83	Incident Detection System Maintenance Contract (in tunnels) (2127)	153	Underway
84	Miscellaneous Paving Repairs (2142)	4,697	Underway
85	Right-of-Way and General Fencing at Various Locations (2144)	836	Underway
86	Study to Evaluate Toll Booth Condition (2094)	90	Underway
87	Evaluate Condition of Deck, Superstructure & Substructures All Facilities (Engineering only) (2083)	1,332	Underway
88	Fatigue Retrofits at Multi Area Structures - Engineering only (Code 2 System Preservation) (2219)	802	Underway
89	On-Call Miscellaneous Shotcrete Repair (2149)	10,004	Underway
90	On-Call Structural Repairs and Miscellaneous Modifications (2124)	5,266	Underway
91	On-Call Structural Repairs and Miscellaneous Modifications (2168)	13,200	Underway
92	On-Call Structural Repairs and Miscellaneous Modifications (2171)	13,200	Underway
93	Open Road Tolling Authority-wide Feasibility Study (2183)	500	Underway
94	Replace Intercom Systems at Toll Collection Booths - Authority wide (2162)	260	Underway
95	Speed Camera Program (2223)	100	Underway
96	Study E-ZPass Disaster Recovery Needs (2225)	100	Underway
97	System Preservation Program Unallocated Fund - Code 2 Projects (1979)	148,647	Underway

SYSTEM PRESERVATION MINOR PROJECTS PROGRAM

MARYLAND TRANSPORTATION AUTHORITY - LINE 35 (cont'd)

ITEM NO.	DESCRIPTION AND IMPROVEMENT TYPE	TOTAL ESTIMATED COST (\$000's)	CONSTRUCTION START
<u>FY 2011 and Prior (cont'd)</u>			
<u>MULTI-AREA (cont'd)</u>			
98	Upgrade and Replace Metal Barriers and Attenuators (1922)	2,652	Underway
99	Energy Lease Program (2078)	10,253	Underway
100	Furnish and Install Radiax Cable within Tunnels (2156)	1,929	Underway
101	Furnish, Deliver, Commission, Warranty and Maintain DMS (Engineering only) (2229)	1,165	Underway
102	Maintenance Contract for Uninterruptible Power Supply Needs (2106)	323	Underway
103	Miscellaneous Roadway Lighting Repairs and Modifications (1913)	4,027	Underway
104	Mitigate Flood at Fairfield Ventilation and Service Buildings - FMT and BHT (0290)	600	Underway
105	Rehabilitation Evaluation Program (Code 1 System Preservation) (2196)	6,573	Underway
106	Replace Navigational Lighting Systems to Francis Scott Key, Nice Memorial Bridge and Bay Bridge (2074)	2,383	Underway
107	Structural On-Call Contract Reserve (Code 1 System Preservation) (2189)	41,351	Underway
108	Study Commercial Vehicle Inspection Facilities at the BHT, KB, FMT (2073)	255	Underway
109	Design & Construct Security Fencing at Shores of Major Bridges (TJH, FSK HWN & Tydings) (2130)	1,850	Spring, 2011
110	Miscellaneous Paving Repairs (Code 1 System Preservation) (2193)	17,425	Spring, 2011
111	On-Call Structural Repairs and Miscellaneous Modifications (2176)	15,700	Spring, 2011
112	Upgrade and Replace Metal Traffic Barriers and Attenuators (Code 1 System Preservation) (2195)	5,723	Spring, 2011
<u>NICE BRIDGE</u>			
113	Fog Detection and Warning System (2154)	60	Underway
114	Renovate Administration Building (1025)	2,103	Underway
115	Deck Replacement/Overlay; Full & Partial Paint - Engineering only (Code 2 System Preservation) (2199)	2,429	Underway
<u>POINT BREEZE</u>			
116	Replace Roof of Garage Building (2108)	659	Spring, 2011

SYSTEM PRESERVATION MINOR PROJECTS PROGRAM

MARYLAND TRANSPORTATION AUTHORITY - LINE 35 (cont'd)

ITEM NO.	DESCRIPTION AND IMPROVEMENT TYPE	TOTAL ESTIMATED COST (\$000's)	CONSTRUCTION START
<u>FY 2011 and Prior (cont'd)</u>			
<u>W. P. LANE BRIDGE</u>			
117	Replace 5KV Cables & Misc. Repairs & Upgrades to the Electrical Systems (0650)	6,905	Complete
118	Study of Eastern Shore Emergency Operations Building (2048)	45	Complete
119	Replace Traffic System Controllers, Signals and Communications and Replace Cameras and Remount (0651)	6,725	Underway
120	Replace Roof on Administration/Maintenance Building (2047)	541	Underway
121	Replace Existing DMS at Bridge and Install New DMS - east of MD 404 (1914)	938	Spring, 2011
122	Replace Underground Tanks at Administration Building & Police West Garage - Bay Bridge (2051)	1,156	Spring, 2011
<u>FY 2012</u>			
<u>F.S.KEY BRIDGE</u>			
123	Upgrade HVAC Systems at Administration Building (2022)	3,230	Spring, 2012
<u>HATEM BRIDGE</u>			
124	Replace Underground with Aboveground Storage Tank - Engineering only (Code 2 System Preservation) (2222)	75	Summer, 2011
<u>KENNEDY HIGHWAY</u>			
125	Drainage Modifications to Hand Boxes (2057)	375	Fall, 2011
126	Expansion Joint Modification of the Tydings Bridge (SB) and Miscellaneous Structural (2182)	5,800	Fall, 2011
<u>MULTI-AREA</u>			
127	Install Security Systems/Video Surveillance at 5 Major Bridges (1966)	7,671	Fall, 2011
128	On-Call Shotcrete Repairs and Miscellaneous Modifications (2227)	13,500	Fall, 2011
129	Zone and Spot Paint Bridges Throughout Facilities - Reserve (Code 1 System Preservation) (2191)	20,842	Spring, 2012

SYSTEM PRESERVATION MINOR PROJECTS PROGRAM

MARYLAND TRANSPORTATION AUTHORITY - LINE 35 (cont'd)

ITEM NO.	DESCRIPTION AND IMPROVEMENT TYPE	TOTAL ESTIMATED COST (\$000's)	CONSTRUCTION START
	<u>FY 2012 (cont'd)</u>		
	<u>NICE BRIDGE</u>		
130	Replace Underground Storage Tanks with Aboveground Storage Tanks (Engineering only) (2200)	52	Summer, 2011
	<u>POINT BREEZE</u>		
131	Study Police Special Operations Facility & Garage (2117)	26	Summer, 2011
	<u>W. P. LANE BRIDGE</u>		
132	Fatigue Retrofits at Bay Bridge - Engineering only (Code 2 System Preservation) (2217)	2,573	Summer, 2011
133	Install 4 Isolation Points and Access Ladder - Eastbound Bay Bridge (2054)	852	Summer, 2011
134	Rehabilitation of Toll Collector Tunnel (2050)	700	Spring, 2012